

OGDEN-HINCKLEY AIRPORT

CONSTRUCT WEST APRON

3909 AIRPORT RD

OGDEN, UT 84405

100% ISSUED FOR BID

JUNE 16, 2025

AIP NO.: 3-49-0024-066-2025

Mead & Hunt
Mead and Hunt, Inc.
999 18th Street,
Suite 2300 South Tower
Denver, CO 80202
phone: 303-825-8844
meadhunt.com



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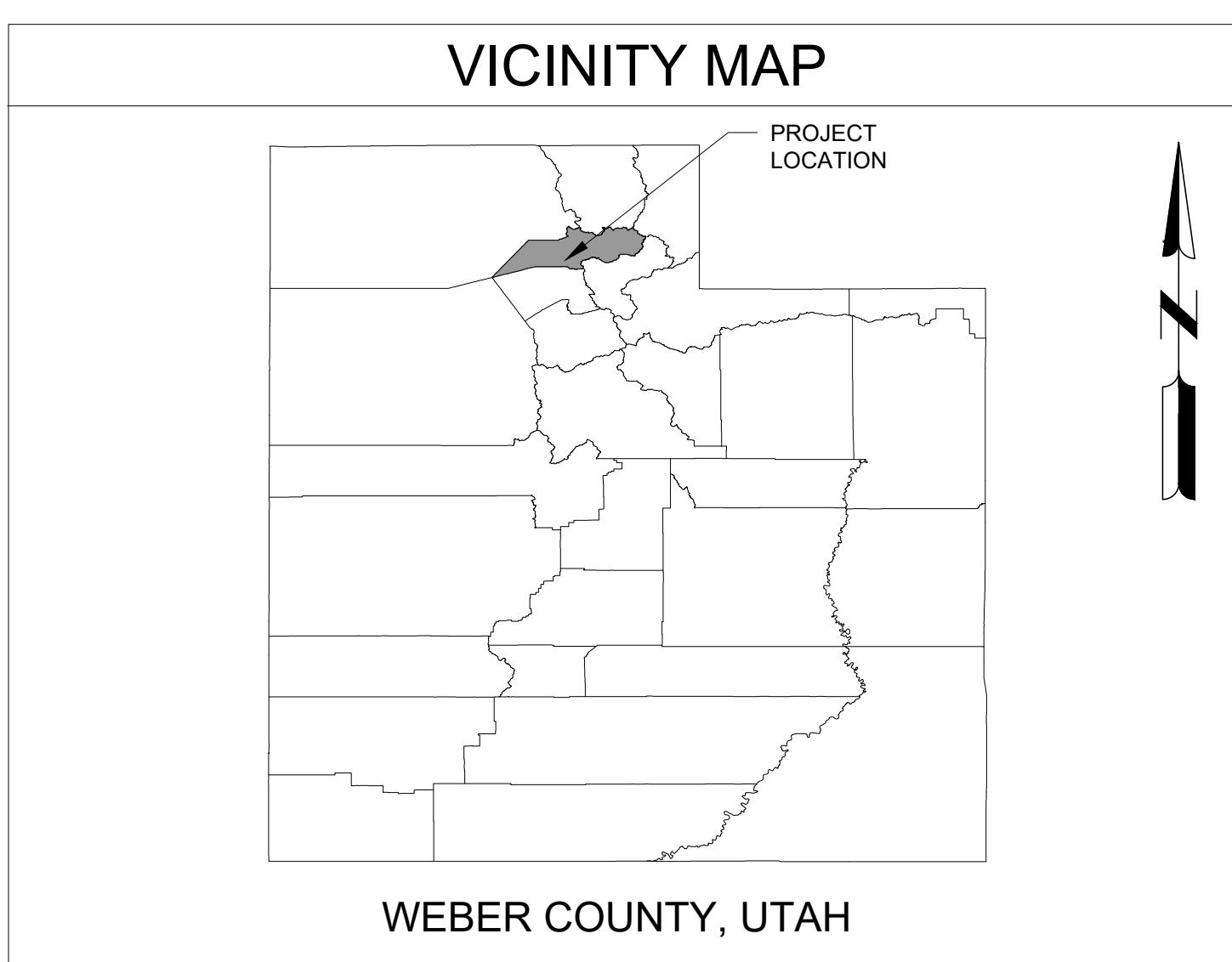
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Know what's below.
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GENERAL NOTES

- IF DURING THE CONSTRUCTION PROCESS, CONDITIONS ARE ENCOUNTERED WHICH COULD INDICATE A SITUATION THAT IS NOT IDENTIFIED IN THE PLANS OR SPECIFICATIONS, THE CONTRACTOR SHALL CONTACT THE RPR IMMEDIATELY.
- ALL REFERENCES TO ANY PUBLISHED STANDARDS SHALL REFER TO THE LATEST REVISION OF SAID STANDARDS, UNLESS SPECIFICALLY STATED OTHERWISE.
- WHENEVER, IN THE CONTRACT DOCUMENTS, THE WORDS "PROVIDE", "FURNISH", "INSTALL", "FURNISH AND INSTALL", OR SIMILAR WORDS ARE USED, IT SHALL BE UNDERSTOOD THAT THE INTENT OF THE CONTRACT DOCUMENTS IS TO PROVIDE FOR THE CONSTRUCTION AND COMPLETION IN EVERY DETAIL THE WORK DESCRIBED. IT IS FURTHER INTENDED THAT THE CONTRACTOR SHALL FURNISH ALL LABOR, SUPERVISION, MATERIALS, EQUIPMENT, TOOLS, TRANSPORTATION, SUPPLIES, TESTING, AND INCIDENTALS REQUIRED TO COMPLETE THE WORK IN ACCORDANCE WITH THE DRAWINGS (PLANS), SPECIFICATIONS, AND TERMS OF THE CONTRACT.
- CONTRACTOR SHALL KEEP A SET OF AS-BUILT DRAWINGS ON-SITE AND MAKE AVAILABLE TO THE RPR AT ALL TIMES. AS-BUILT SET SHALL BE SUBMITTED TO THE RPR AT THE COMPLETION OF THE JOB. CONTRACTOR SHALL BE RESPONSIBLE FOR RECORDING ALL AS-BUILT INFORMATION DURING THE PROJECT. THE CONTRACTOR SHALL NOTE, AND BRING TO THE RPR'S ATTENTION, ANY DISCREPANCIES BETWEEN THE CONTRACT DOCUMENTS AND ACTUAL FIELD CONDITIONS.
- ALL DAMAGE TO UTILITIES, PAVEMENT, EQUIPMENT, OR STRUCTURES FROM CONSTRUCTION ACTIVITIES SHALL BE IMMEDIATELY REPORTED TO THE RPR. THE RPR SHALL DETERMINE WHETHER REPAIR OR REPLACEMENT IS NECESSARY. ALL REPAIR METHODS SHALL BE SUBMITTED TO THE RPR FOR REVIEW AND APPROVAL PRIOR TO INITIATING THE WORK. REPAIRS SHALL BE MADE TO THE APPROVAL OF THE RPR AND AT NO ADDITIONAL COST TO THE SPONSOR.
- THE CONTRACTOR SHALL PROVIDE WORKMANSHIP AND MATERIALS THAT ARE OF GOOD QUALITY AND COMPLY WITH THE REQUIREMENTS OF THE CONTRACT DOCUMENTS.
- THE CONTRACTOR SHALL PROVIDE WORK, EQUIPMENT AND MATERIALS THAT COMPLY WITH FAA REQUIREMENTS, NATIONAL ELECTRICAL CODE, NATIONAL ELECTRICAL SAFETY CODE, AND ALL LOCAL CODES.
- CONTRACTOR SHALL PROVIDE THE NECESSARY NUMBER OF RADIOS FOR HIS/HER WORKFORCE. RADIOS SHALL BE INSPECTED DAILY AND REMAIN CHARGED AND IN GOOD WORKING CONDITION AT ALL TIMES.
- THIRTY (30) DAYS PRIOR TO THE BEGINNING OF WORK, THE CONTRACTOR SHALL SUBMIT A WORK SCHEDULE CONSISTENT WITH THE PHASING PLAN STATED IN THE DESIGN.
- THE CONTRACTOR SHALL HAVE A MINIMUM OF ONE (1) CURRENT COPY OF THE APPROVED PLANS (INCLUDING ANY CHANGE ORDERS, SUPPLEMENTAL AGREEMENTS, FIELD DIRECTIVES, ETC.), ONE (1) CURRENT COPY OF THE APPROPRIATE STANDARDS AND SPECIFICATIONS, AND A COPY OF ANY PERMITS AND EXTENSION AGREEMENTS NEEDED FOR THE JOB, ON SITE AT ALL TIMES.
- DIMENSIONING FOR LAYOUTS AND CONSTRUCTION ARE NOT TO BE SCALED FROM ANY DRAWINGS. IF PERTINENT DIMENSIONS ARE NOT SHOWN, CONTACT THE RPR FOR CLARIFICATION.

UTILITIES

- THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES, EXISTING DUCT BANKS, CIRCUITING, AND STRUCTURES AS SHOWN ON THESE PLANS, IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES AND, WHERE POSSIBLE, BASED ON AVAILABLE INFORMATION OR MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED UPON AS BEING EXACT OR COMPLETE, NOR IS IT WARRANTED THAT ALL ITEMS ARE SHOWN. THE EXISTING UTILITY LOCATIONS SHOWN ON THE PLANS SHALL NOT BE SCALED FOR EXACT LOCATIONS.

SUBMITTALS

- THE CONTRACTOR SHALL SUBMIT A DETAILED LISTING OF ALL SUBMITTALS (E.G., MIX DESIGNS, MATERIAL CERTIFICATION, AND PRODUCT INFORMATION) AND SHOP DRAWINGS REQUIRED BY THE TECHNICAL SPECIFICATIONS.
- THE CONTRACTOR SHALL REVIEW THE CONTRACT DOCUMENTS SECTION 100-05 FOR SUBMITTAL SCHEDULE REQUIREMENTS.
- THE CONTRACTOR SHALL PROVIDE MATERIAL SUBMITTALS FOR THE RPR'S APPROVAL AT LEAST TEN (10) DAYS PRIOR TO ORDERING.

SITE ACCESS

- DURING CONSTRUCTION, THE CONTRACTOR SHALL MINIMIZE DISTURBANCES TO ALL CONSTRUCTION AREAS AND ACCESS ROUTES. THIS INCLUDES EQUIPMENT AND VEHICULAR RUTS CREATED IN ANY PAVEMENTS, ANY HAUL/ACCESS ROADS, OR ANY INFIL/SAFETY AREAS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO EXISTING FACILITIES OR ROADS. REPAIRS SHALL BE MADE AT NO ADDITIONAL COST TO THE SPONSOR AND TO THE SATISFACTION OF THE RPR.
- CONTRACTOR SHALL EXAMINE THE EXISTING PAVEMENTS THAT WILL BE USED FOR HAULING OF MATERIAL AND EQUIPMENT AND DETERMINE THE PAVEMENT'S ABILITY TO WITHSTAND CONTRACTOR OPERATIONS WITHOUT CAUSING DAMAGE TO THE PAVEMENT. ANY DAMAGE CAUSED BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR TO THE APPROVAL OF THE RPR AND AT NO ADDITIONAL COST TO THE SPONSOR.
- AT NO TIME SHALL LIGHT PLANTS BE LEFT RUNNING WHEN CONSTRUCTION OPERATIONS ARE NOT IN PROCESS.
- ALL CONTRACTOR EMPLOYEES SHALL BE REQUIRED TO PARK IN THE CONTRACTOR'S DESIGNATED STAGING AREA ONLY AND SHALL BE DRIVEN TO THE PROJECT SITE BY DESIGNATED CONSTRUCTION VEHICLES. ALL DRIVERS SHALL COMPLETE THE REQUIRED AIRFIELD DRIVING TRAINING BEFORE BEING ALLOWED TO DRIVE ON THE AIRPORT.
- THE CONTRACTOR SHALL BE AWARE THAT OTHER CONSTRUCTION MAY BE ACTIVE DURING THIS PROJECT. COORDINATION WILL BE REQUIRED WITH AIRPORT STAFF AND VARIOUS CONTRACTORS THROUGH THE RPR.
- CONTRACTOR SHALL PROVIDE TEMPORARY FENCING WITH AT LEAST TWO GATES AROUND THE CONSTRUCTION YARD.

SAFETY

- DURING CONSTRUCTION, THE CONTRACTOR SHALL COMPLY WITH FAA ADVISORY CIRCULAR (AC) 150/5370-2G, "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION".
- THE CONTRACTOR SHALL REVIEW THE CONSTRUCTION SAFETY AND PHASING PLAN (CSP) CONTAINED IN THE CONTRACT DOCUMENTS. ADVISORY CIRCULAR (AC) 150/5370-2G, "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION" REQUESTS THE CONTRACTOR TO PREPARE A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) PRIOR TO NTP FOR APPROVAL BY THE RPR.
- ALL VEHICLES AND EQUIPMENT WORKING REGULARLY ON THE PROJECT SITE SHALL BE REQUIRED TO BE EQUIPPED WITH STANDARD FAA MARKINGS PER FAA ADVISORY CIRCULAR 150/5210-5 OR BE ESCORTED BY A PROPERLY MARKED VEHICLE. A FLASHING BEACON MAY BE USED AT ANY TIME. FAILURE TO PROVIDE SUCH MARKINGS OR ESCORT FOR ANY EQUIPMENT INSIDE THE AIRPORT PERIMETER FENCE WILL PRECLUDE THAT EQUIPMENT FROM OPERATING ON THE PROJECT. DELAYS CAUSED DUE TO LACK OF CONFORMANCE SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. BACKUP ALARMS SHALL BE ADJUSTED FOR SURROUNDING NOISE LEVELS.
- RESERVED.
- RESERVED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL ASPECTS OF SAFETY INCLUDING, BUT NOT LIMITED TO, EXCAVATION, TRENCHING, SHORING, TRAFFIC CONTROL, AND SECURITY.

PERMITTING

- THE CONTRACTOR SHALL SUBMIT A COPY OF ALL PERMITS REQUIRED FOR THE PROJECT TO THE RPR, FOR HIS REVIEW.

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GENERAL ABBREVIATIONS

AC	ASPHALT CONCRETE
ALT	ALTERNATE
AOA	AIR OPERATIONS AREA
APPROX	APPROXIMATE
ARFF	AIRCRAFT RESCUE AND FIRE FIGHTING
ATCT	AIR TRAFFIC CONTROL TOWER
ATO	AIR TRAFFIC ORGANIZATION
BLDG	BUILDING
CP	CONTROL POINT
CSPP	CONSTRUCTION SAFETY AND PHASING PLAN
DEG	DEGREE
DIA	DIAMETER
DIM	DIMENSION
(E)	EXISTING
E	EASTING
ELEV	ELEVATION
(F)	FUTURE
FAA	FEDERAL AVIATION ADMINISTRATION
FL	FLOW LINE
FS	FINISH SURFACE
FT	FEET
GB	GRADE BREAK
GSCA	GLIDE SLOPE CRITICAL AREA
HMA	HOT MIX ASPHALT
IN	INCHES
L	LENGTH
LCA	LOCALIZER CRITICAL AREA
LF	LINEAL FEET
MIN	MINIMUM
MALSR	MEDIUM APPROACH LIGHTING SYSTEM WITH RAIL
NO. OR #	NUMBER
NOTAM	NOTICE TO AIR MISSIONS
N	NORTHING
NAVD	NORTH AMERICAN DATUM
NGS	NATIONAL GEODETIC SURVEY
NIC	NOT IN CONTRACT
NTS	NOT TO SCALE
OFF	OFFSET
OFZ	OBJECT FREE ZONE
(P)	PROPOSED
PAPI	PRECISION APPROACH PATH INDICATOR
PACS	PRIMARY AIRPORT CONTROL STATION
PCC	PORTLAND CEMENT CONCRETE
PT	POINT OF TANGENCY
QTY	QUANTITY
R	RADIUS
RD	ROAD
REQ	REQUIRED
ROFA	RUNWAY OBJECT FREE AREA
RPZ	RUNWAY PROTECTION ZONE
RSA	RUNWAY SAFETY AREA
RTR	REMOTE TRANSMITTER/RECEIVER
RVZ	RUNWAY VISIBILITY ZONE
RW OR RWY	RUNWAY
SF	SQUARE FOOT
SIDA	SECURITY IDENTIFICATION DISPLAY AREA
SPCD	SAFETY PLAN COMPLIANCE DOCUMENT
STA	STATION
STD	STANDARD
TW OR TWY	TAXIWAY
TOFA	TAXIWAY OBJECT FREE AREA
TSA	TAXIWAY SAFETY AREA
TYP	TYPICAL
W/	WITH
W/O	WITHOUT

PATTERNS

LEGEND:	
	AGGREGATE BASE COURSE
	AGGREGATE SUBBASE
	ASPHALT, PROPOSED
	CONCRETE, PROPOSED
	GROUND, EXISTING
	GROUND, PROPOSED
	WATER QUALITY BASIN
	HYDROSEED
	CLEAR AND GRUB
	CONTRACTOR STAGING AREA
	DIRT DISPOSAL AREA
	STOCKPILE AREA
	VEHICLE TRACKING CONTROL

REMOVAL

LEGEND:	
	ABANDON
	ABANDON CONDUIT
	ABANDON ELECTRIC, UNDERGROUND
	ABANDON GAS, UNDERGROUND
	ABANDON SANITARY SEWER
	ABANDON STORM SEWER
	ABANDON WATER
	REMOVE
	REMOVE CONDUIT
	REMOVE ELECTRIC, UNDERGROUND
	REMOVE ELECTRIC, OVERHEAD
	REMOVE FENCE
	REMOVE FIBER OPTIC CABLE
	REMOVE GAS LINE
	REMOVE SANITARY SEWER
	REMOVE STORM SEWER
	REMOVE TELEPHONE
	REMOVE WATER LINE
	SAW CUT PAVEMENT

LINES

LEGEND:	
	ASOS CRITICAL AREA
	AIRCRAFT PARKING LIMIT
	AIRPORT OPERATION AREA
	ASOS CRITICAL AREA
	APPROACH SURFACE
	BUILDING RESTRICTION LINE
	DEPARTURE RUNWAY PROTECTION ZONE
	DEPARTURE SURFACE
	FAA
	GLIDESLOPE CRITICAL AREA
	GROUND CONTROL
	APPROACH
	GROUNDS WIRE
	OBJECT FREE AREA
	OBJECT FREE ZONE
	RUNWAY GUARD LIGHTS
	RUNWAY OBJECT FREE AREA
	RUNWAY PROTECTION ZONE
	RUNWAY RESTRICTED AREA
	RUNWAY SAFETY AREA
	RUNWAY WORK AREA
	SECURITY IDENTIFICATION DISPLAY AREA
	TAXIWAY OBJECT FREE AREA
	TAXIWAY SAFETY AREA
	TEMPORARY TAXIWAY SAFETY AREA
	EXISTING CONTOUR LINES
	PROPOSED CONTOUR LINES
	SEDIMENT LOG
	SILT FENCE
	CONDUIT, GENERIC
	GAS
	ELECTRIC, OVERHEAD
	ELECTRIC, UNDERGROUND
	FENCE - PERMANENT AOA
	FIBER OPTIC CABLE
	FORCE MAIN
	SANITARY SEWER
	SIGNAL CABLE, UNDERGROUND
	STORM SEWER / CULVERT
	TELEPHONE, OVERHEAD
	TELEPHONE, UNDERGROUND
	TV CABLE
	UNDERDRAIN
	WATER
	EASEMENT
	GRADE BREAK
	GRADING LIMITS
	PROPERTY LINE
	SWALE
	DISTURBANCE LIMITS
	GRADING LIMITS

SYMBOLS

LEGEND:	
	FIRE HYDRANT
	GAS MANHOLE
	GAS METER
	GAS VALVE
	HANDHOLE, GENERIC
	MARKER, CABLE
	MARKER, DUCT
	PIEZOMETER
	SANITARY SEPTIC TANK VALVE
	SANITARY SEWER LIFT STATION
	SANITARY SEWER MANHOLE
	SANITARY SEWER VALVE
	STORM INLET, ROUND
	(E) STORM INLET, SQUARE
	STORM FLARED END SECTION
	STORM SEWER MANHOLE
	STORM WATER QUALITY MANHOLE
	VALVE MANHOLE
	WATER MANHOLE
	WATER METER
	WATER PRESSURE VALVE
	WATER REDUCER
	WATER VALVE
	DRAINAGE FLOW DIRECTION
	INLET PROTECTION
	BENCHMARK
	CONTROL POINT
	734.97'
	738.76'
	3:1
	2%
	GATE
	ACCESS POINT
	HAUL ROUTE
	TAXIWAY EDGE REFLECTOR
	TAXIWAY EDGE LIGHTS
	RUNWAY EDGE LIGHTS
	FLOWLINE

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LEGEND AND
ABBREVIATIONS

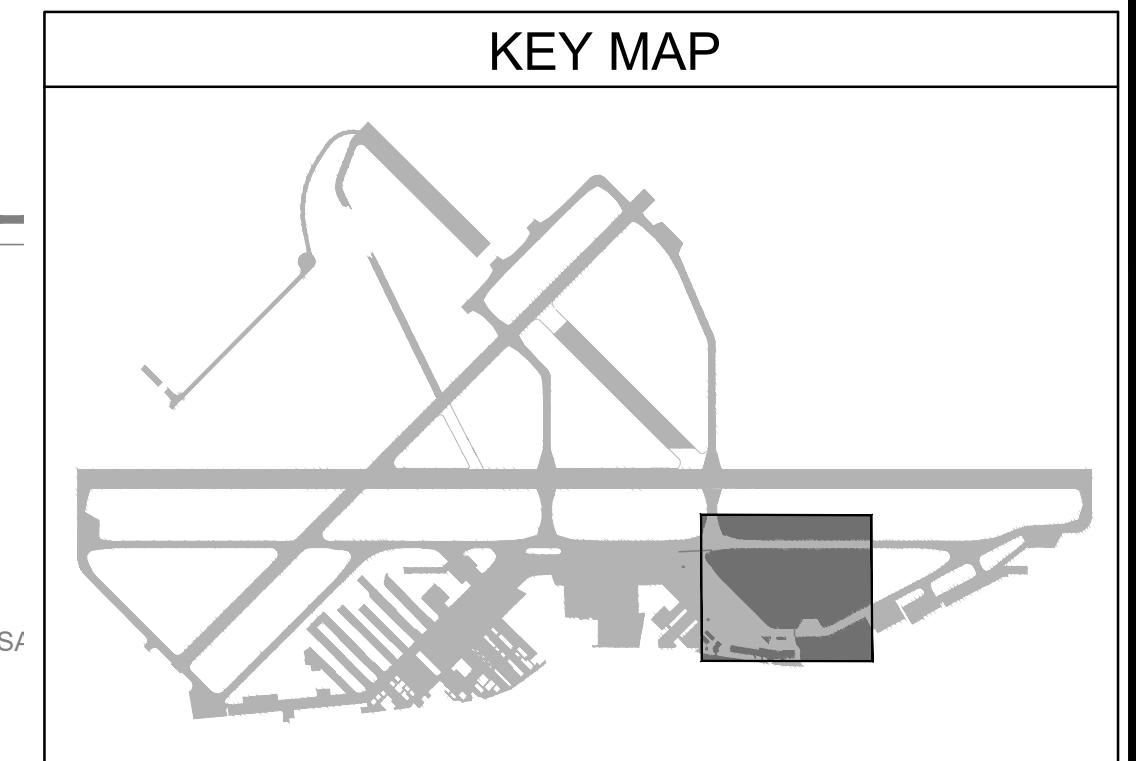
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KEY MAP



LEGEND

	PROPOSED PCC
	PROPOSED AC
	TEMPORARY CONTRACTOR'S STAGING AREA
	TEMPORARY STOCKPILE AREA
	VEHICLE TRACKING CONTROL AREA
	BID ALT. 1 PAVING LIMITS

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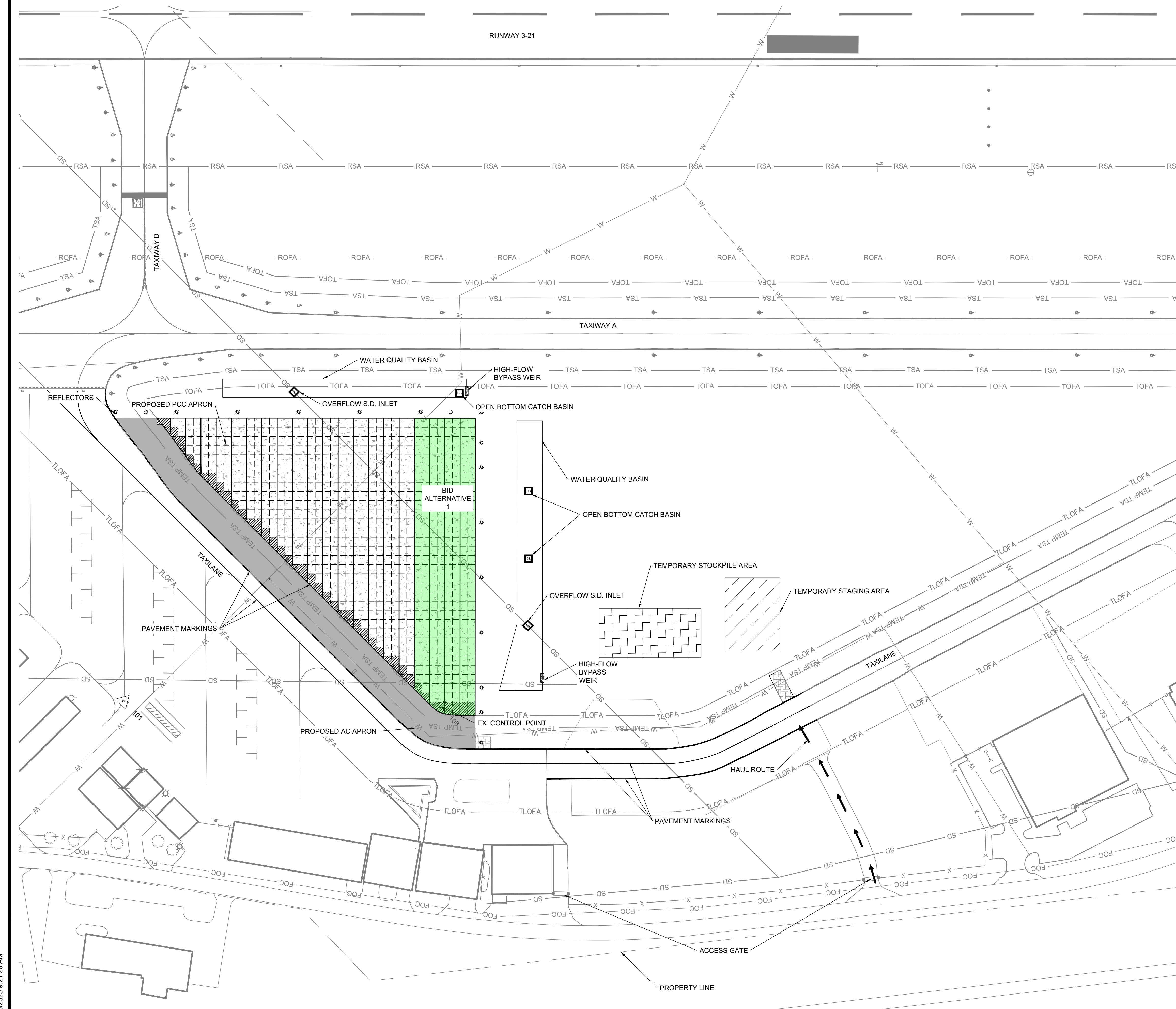
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SHEET CONTENTS
PROJECT LAYOUT
PLAN

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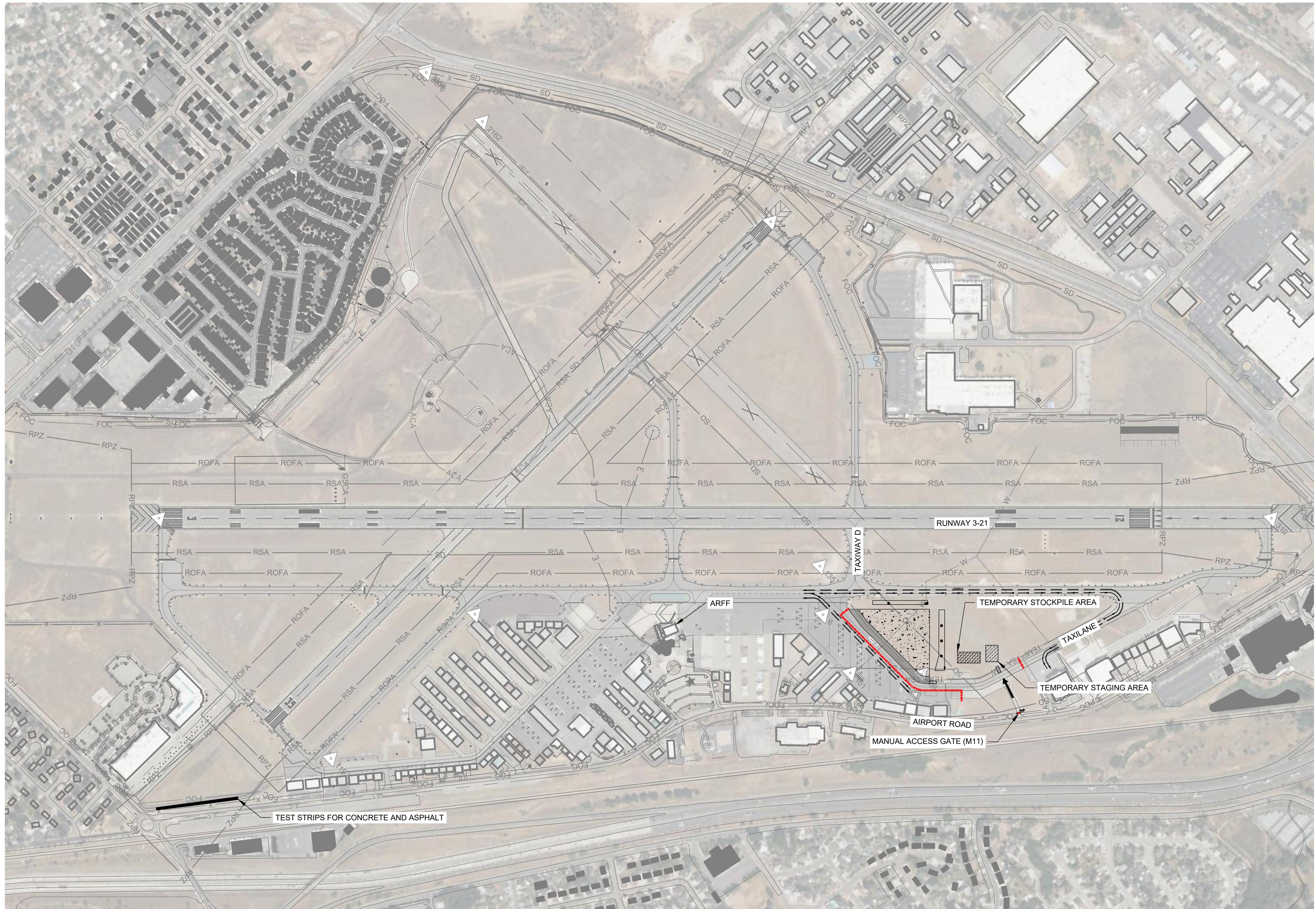
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LEGEND

	PROPOSED PCC
	PROPOSED AC
	TEMPORARY CONTRACTOR'S STAGING AREA
	TEMPORARY STOCKPILE AREA
	VEHICLE TRACKING CONTROL
	HAUL ROUTE
	ARFF ROUTE
	TAXI ROUTE
	INTERLOCKED LOW-PROFILE BARRICADES
	FLAG MAN
	GATE
	ACCESS POINT
	RUNWAY SAFETY AREA
	RUNWAY OBJECT FREE AREA
	TAXIWAY SAFETY AREA
	TAXIWAY OBJECT FREE AREA
	TAXILANE OBJECT FREE AREA

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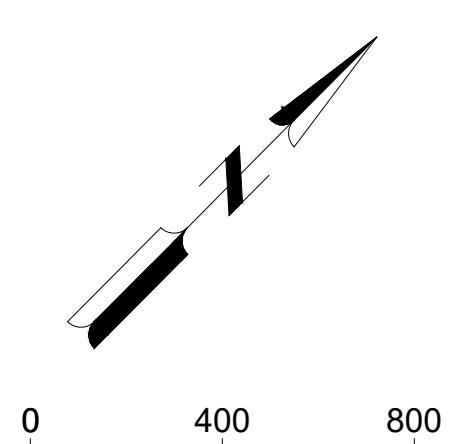
PHASE	WORK HOURS	WORK	PAVEMENT CLOSURES DUE TO THE WORK	OPERATIONAL THRU TRAFFIC DUE TO THE WORK	CALENDAR DAYS	CONCURRENT CLOSURE ALLOWABLES	WORK AREAS NOTES
MOB	0630-1830	• ESTABLISH CONTRACTOR'S YARD • PROVIDE SUBMITTALS	NONE		30 DAYS	NONE	DO NOT IMPEDE ARFF ACCESS.
1	0700-0800	• SUBGRADE PREPARATION • SUBBASE AND AGG BASE COURSE • AC BASE PAVEMENT	TAXILANE (AS SHOWN IN THE SHEET)	NORTH APRON SOUTH OF TAXILANE	60 DAYS	NONE	DO NOT IMPEDE ARFF ACCESS.
2	NONE	• NO WORK (SHUTDOWN)	TAXILANE (AS SHOWN IN THE SHEET)	NORTH APRON SOUTH OF TAXILANE	90 DAYS	NONE	MAINTAIN BARRICADES.
3	0700-0800	• AC SURFACE PAVEMENT • PCC PAVEMENT • APPLY PAVEMENT MARKING	TAXILANE (AS SHOWN IN THE SHEET)	NORTH APRON SOUTH OF TAXILANE	60 DAYS	NONE	DO NOT IMPEDE ARFF ACCESS.

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Sheet Contents
OVERALL CONSTRUCTION SAFETY AND PHASING

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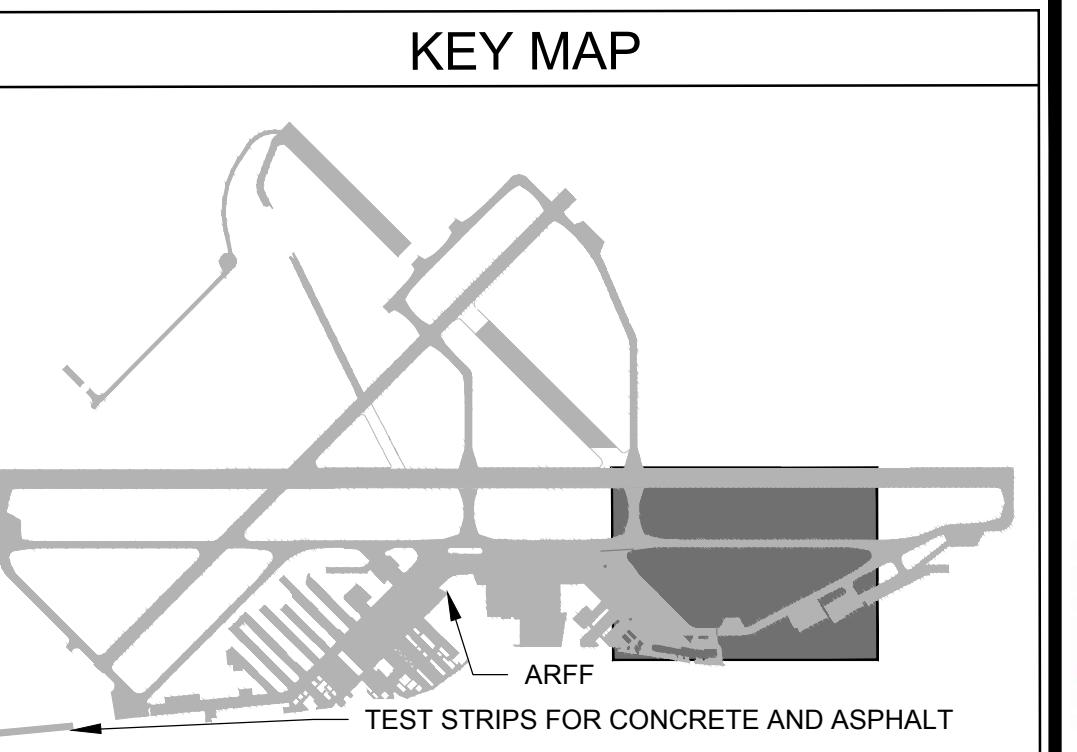




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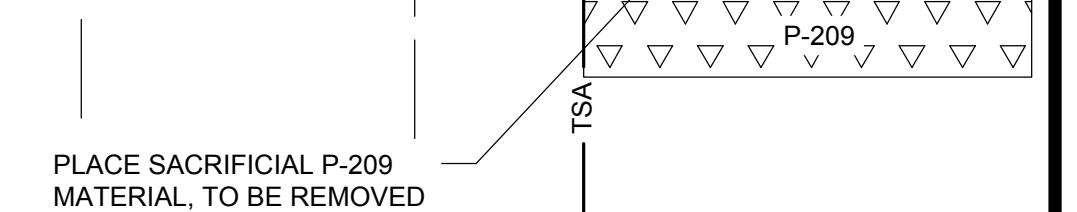


LEGEND

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	TEMPORARY CONTRACTOR'S STAGING AREA
	TEMPORARY STOCKPILE AREA
	VEHICLE TRACKING CONTROL
	HAUL ROUTE
	ARFF ROUTE
	TAXI ROUTE
	INTERLOCKED LOW-PROFILE BARRICADES
	FLAG MAN
	GATE
	ACCESS POINT
	RUNWAY SAFETY AREA
	RUNWAY OBJECT FREE AREA
	TAXIWAY SAFETY AREA
	TAXIWAY OBJECT FREE AREA
	TAXILANE OBJECT FREE AREA

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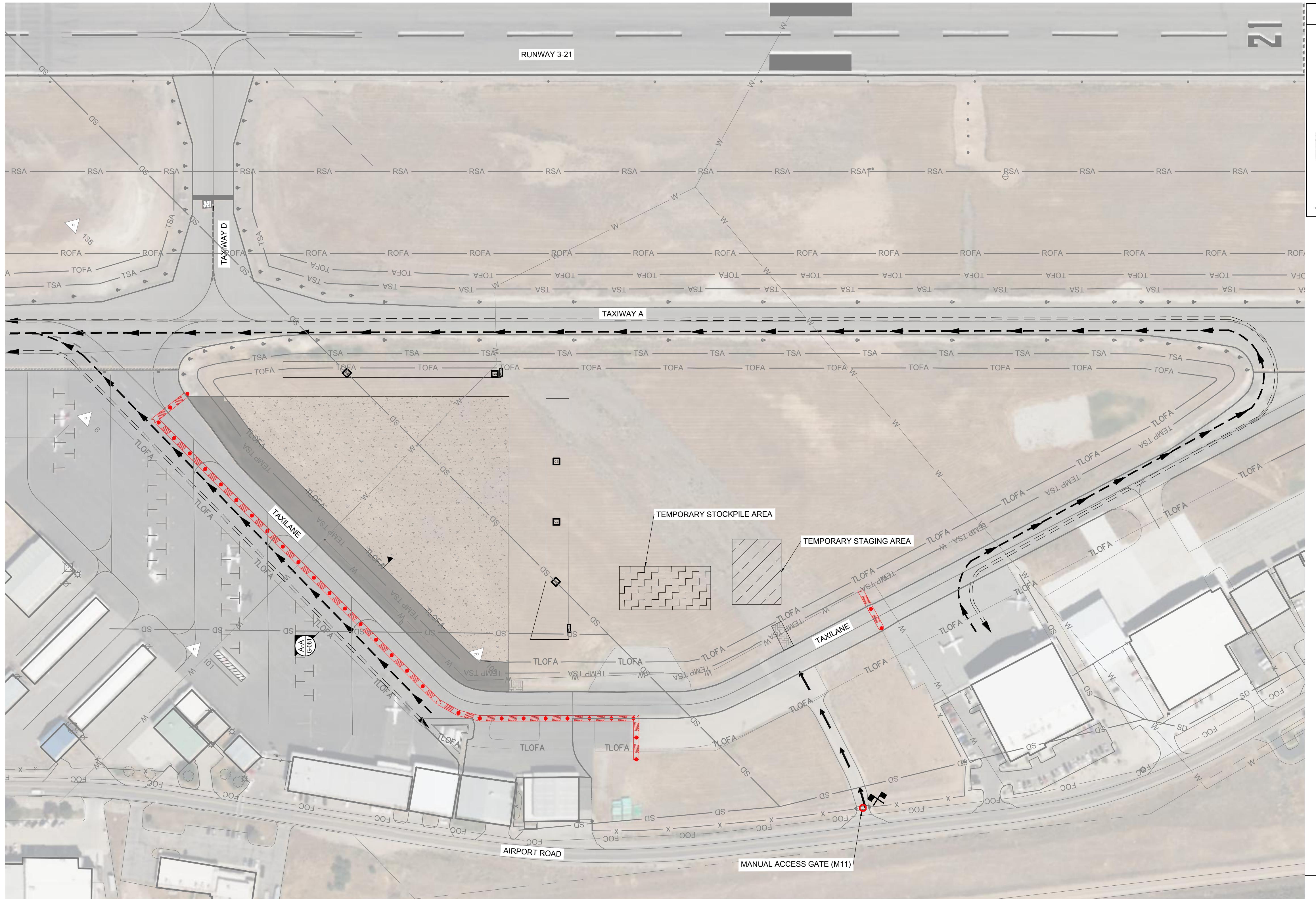
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SAFETY AND PHASING:
PHASE 1

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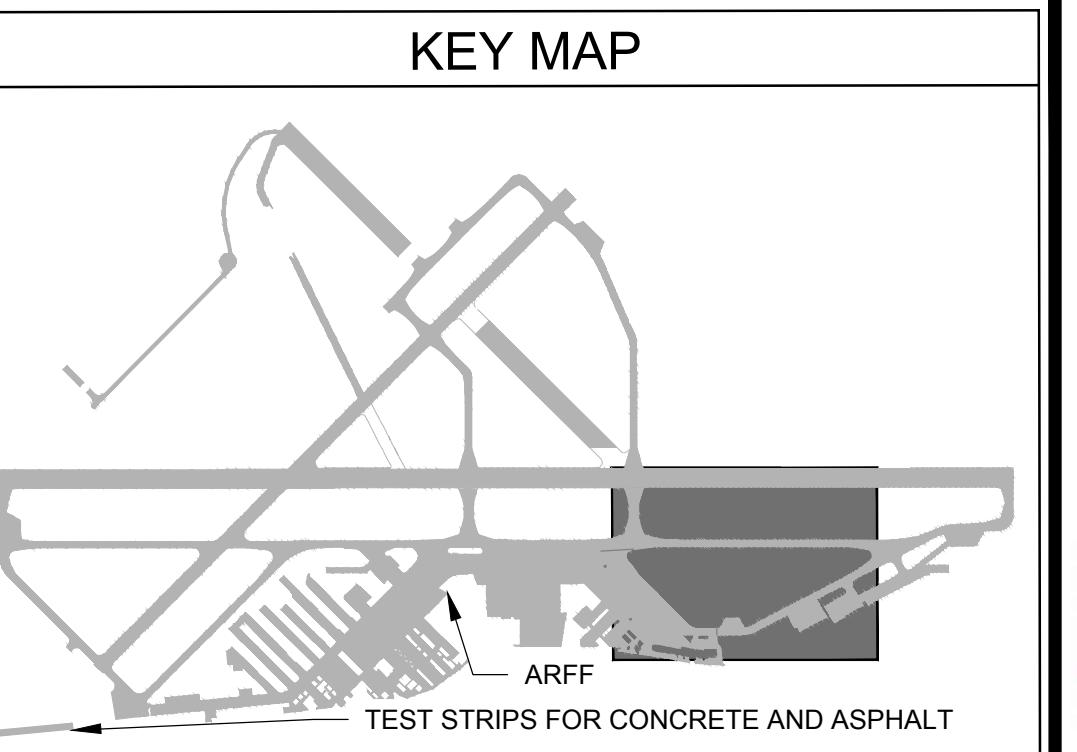
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MOB	0630-1830	• ESTABLISH CONTRACTOR'S YARD • PROVIDE SUBMITTALS	NONE			NONE	DO NOT IMPEDE ARFF ACCESS.
1	0700-0800	• SUBGRADE PREPARATION • SUBBASE AND AGG BASE COURSE • AC BASE PAVEMENT	TAXILANE (AS SHOWN IN THE SHEET)	NORTH APRON SOUTH OF TAXILANE		NONE	DO NOT IMPEDE ARFF ACCESS.
2	NONE	• NO WORK (SHUTDOWN)	TAXILANE (AS SHOWN IN THE SHEET)	NORTH APRON SOUTH OF TAXILANE		NONE	MAINTAIN BARRICADES.
3	0700-0800	• AC SURFACE PAVEMENT • PCC PAVEMENT • APPLY PAVEMENT MARKING	TAXILANE (AS SHOWN IN THE SHEET)	NORTH APRON SOUTH OF TAXILANE		NONE	DO NOT IMPEDE ARFF ACCESS.



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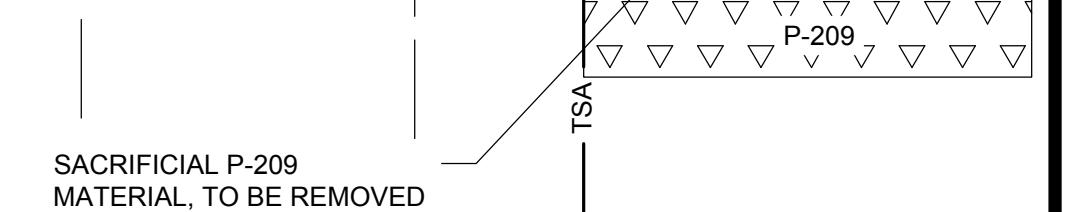


LEGEND

	PROPOSED AC
	TEMPORARY CONTRACTOR'S STAGING AREA
	TEMPORARY STOCKPILE AREA
	VEHICLE TRACKING CONTROL
	HAUL ROUTE
	ARFF ROUTE
	TAXI ROUTE
	INTERLOCKED LOW-PROFILE BARRICADES
	FLAG MAN
	GATE
	ACCESS POINT
	RUNWAY SAFETY AREA
	RUNWAY OBJECT FREE AREA
	TAXIWAY SAFETY AREA
	TAXIWAY OBJECT FREE AREA
	TAXILANE OBJECT FREE AREA

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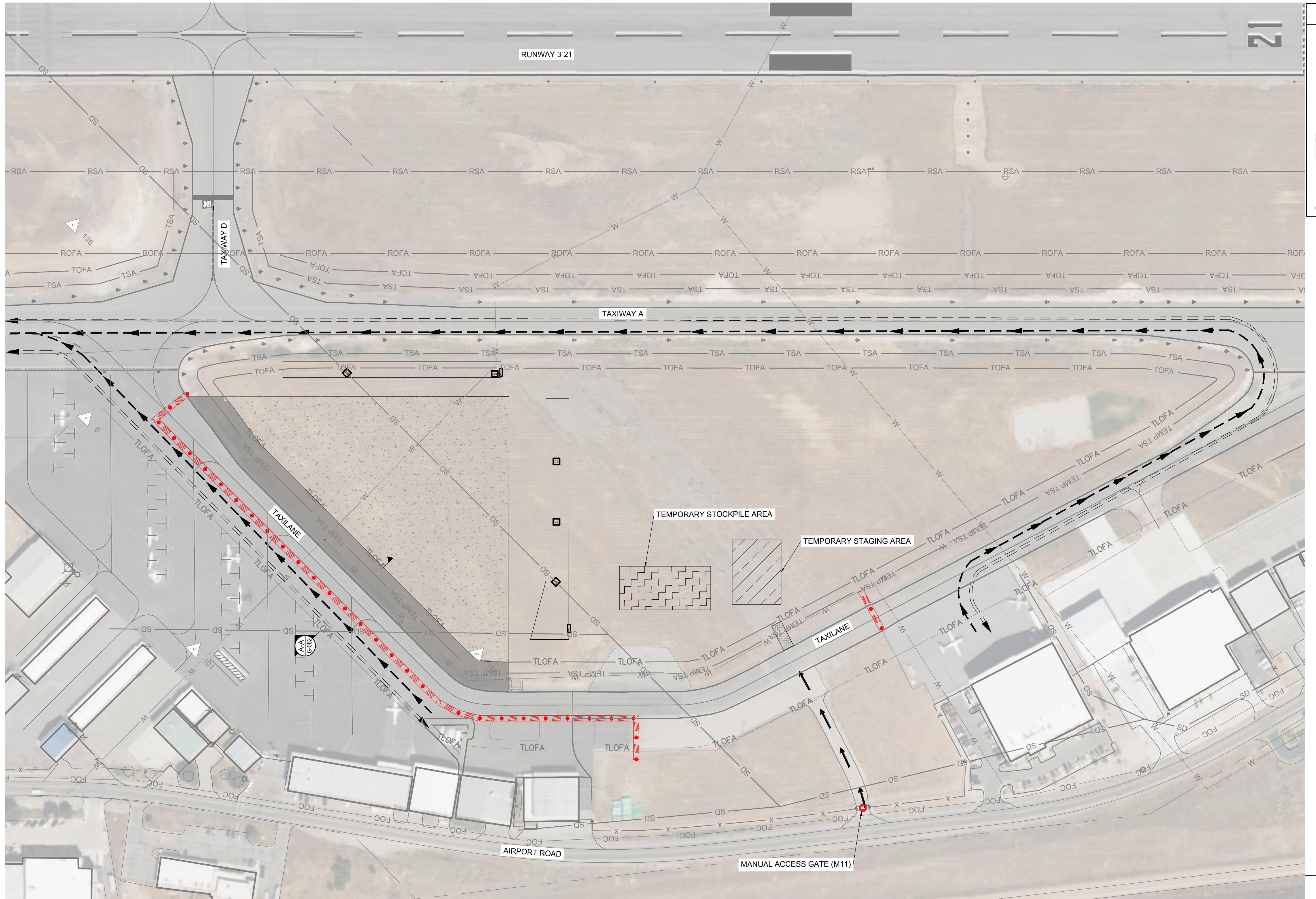
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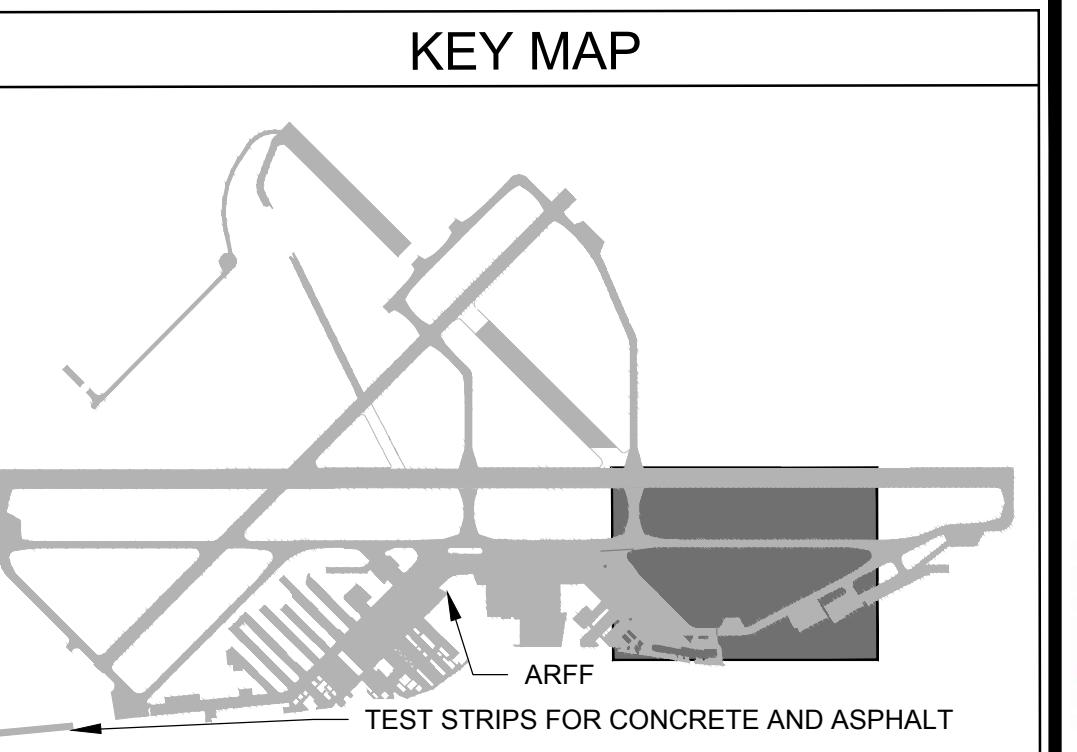
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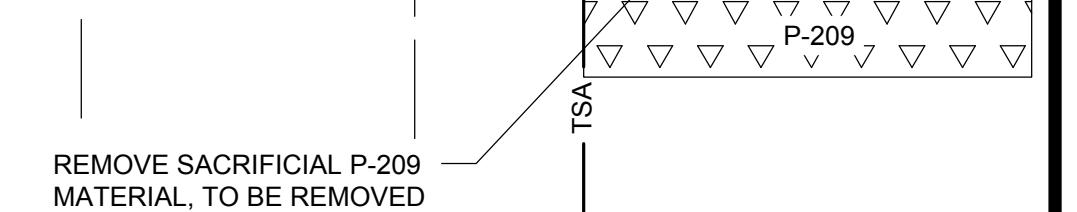


LEGEND

	PROPOSED PCC
	PROPOSED AC
	TEMPORARY CONTRACTOR'S STAGING AREA
	TEMPORARY STOCKPILE AREA
	VEHICLE TRACKING CONTROL
	HAUL ROUTE
	ARFF ROUTE
	TAXI ROUTE
	INTERLOCKED LOW-PROFILE BARRICADES
	FLAG MAN
	GATE
	ACCESS POINT
	RUNWAY SAFETY AREA
	RUNWAY OBJECT FREE AREA
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	TAXIWAY OBJECT FREE AREA
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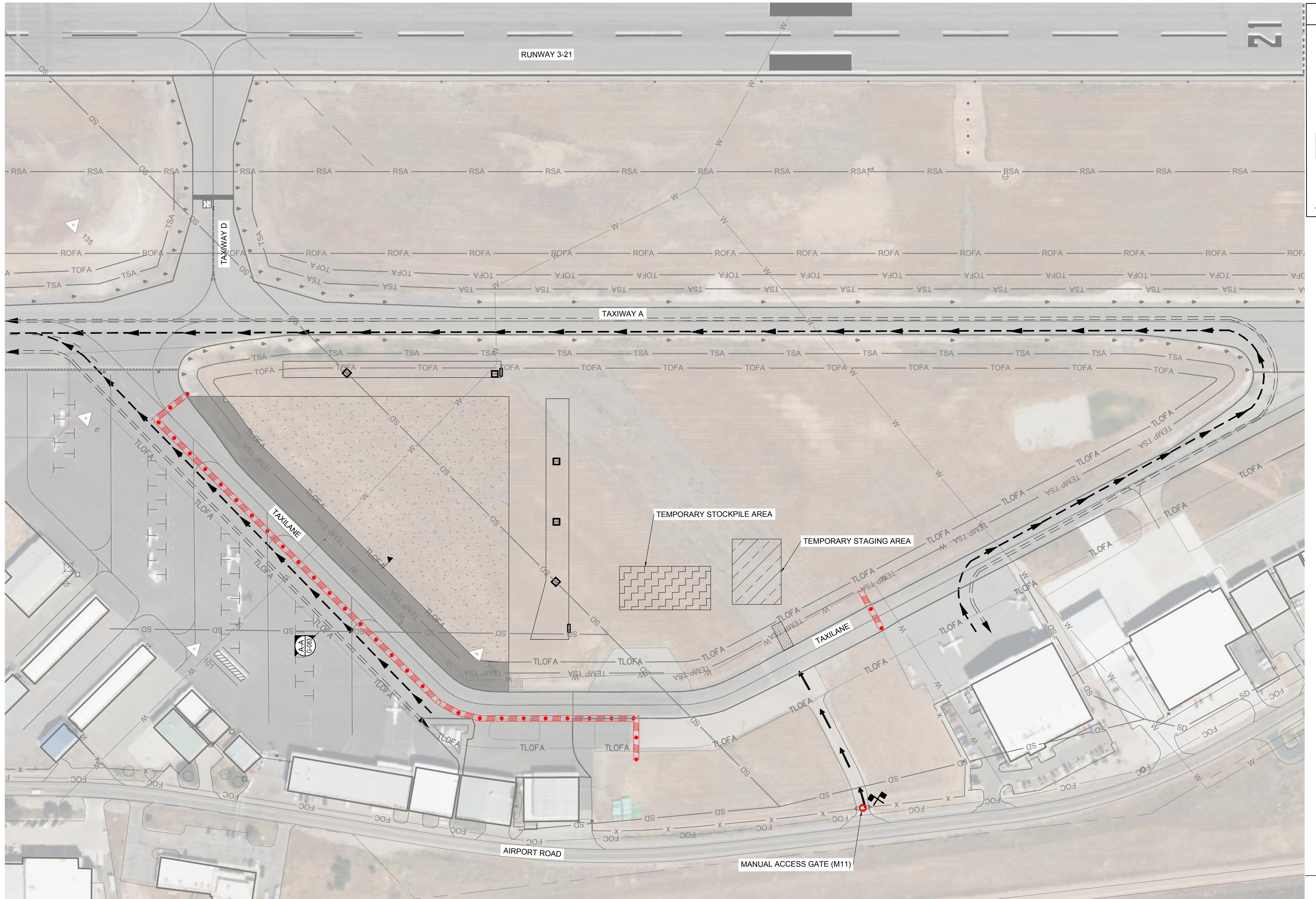
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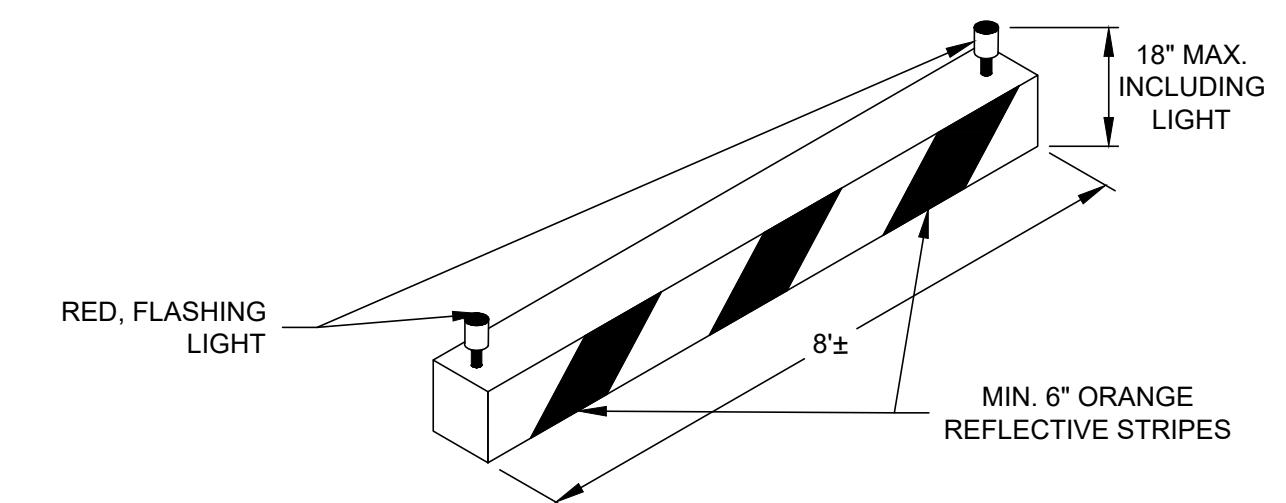
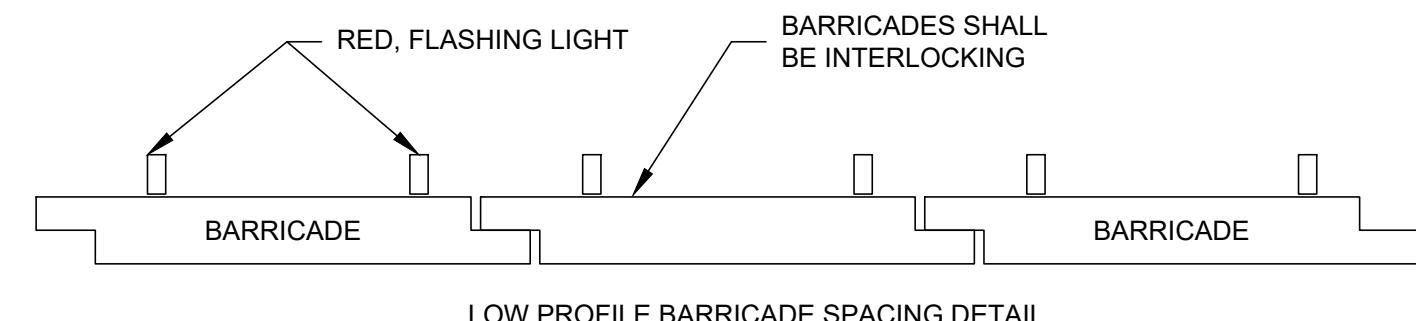
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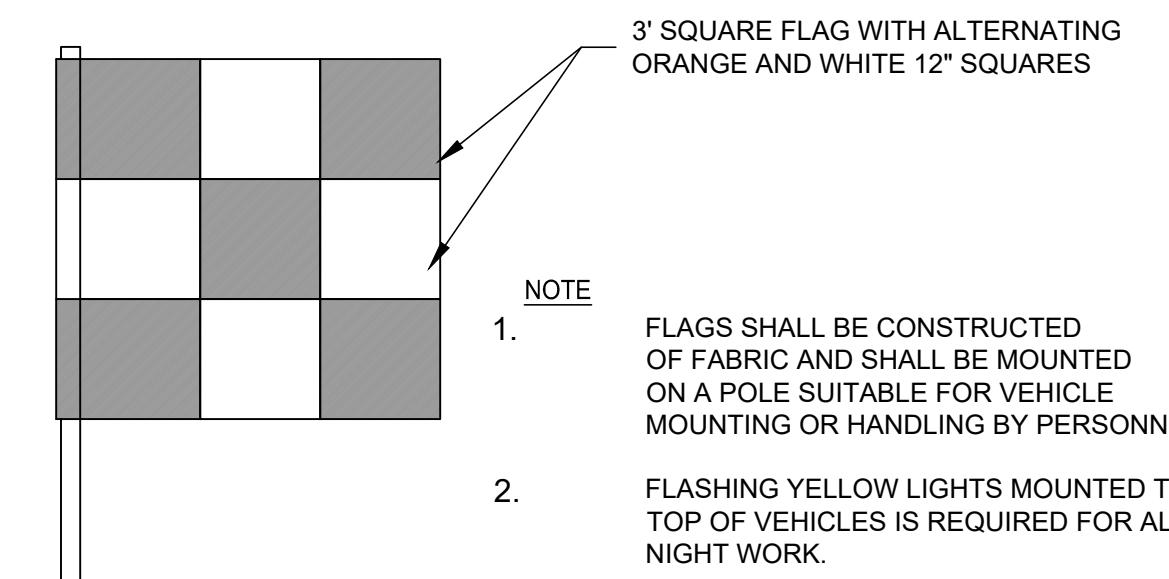


NOTES:

1. BARRICADE FLASHERS MUST BE FUNCTIONAL AND DAMAGED BARRICADES SHALL NOT BE USED.
2. BARRICADE TO BE FILLED WITH WATER AND CONSTANTLY MONITORED.
3. LOW PROFILE BARRICADE TO MEET CURRENT EDITION OF FAA AC 150/5370-2 < 18" HEIGHT.
4. UV RESISTANT POLYETHYLENE WATER FILLED, < 8' LENGTH, ORANGE AND WHITE, WITH FLASHING SOLAR AND BATTERY POWERED LIGHTS.

1 LOW-PROFILE BARRICADE

NOT TO SCALE



NOTE

1. FLAGS SHALL BE CONSTRUCTED OF FABRIC AND SHALL BE MOUNTED ON A POLE SUITABLE FOR VEHICLE MOUNTING OR HANDLING BY PERSONNEL.
2. FLASHING YELLOW LIGHTS MOUNTED TO TOP OF VEHICLES IS REQUIRED FOR ALL NIGHT WORK.

2 CONSTRUCTION VEHICLE CHECKERED FLAG DISPLAY

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Know what's below.
Call before you dig.

GENERAL:

A. THE CONTRACTOR IS ADVISED THAT ALL EMPLOYEES INSIDE THE AOA FENCE MUST CONFORM WITH ALL AIRFIELD SECURITY REQUIREMENTS. THE CONTRACTOR SHALL CONTACT THE AIRPORT FOR ALL REQUIREMENTS. ANY CONTRACTOR EMPLOYEE NOT CONFORMING TO AIRFIELD OR FAA REQUIREMENTS WILL BE ASKED TO LEAVE THE PROJECT AND NOT RETURN FOR THE REMAINDER OF THE PROJECT.

SAFETY PLAN COMPLIANCE DOCUMENT

A. THE CONTRACTOR SHALL PREPARE A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) DESCRIBING HOW THE REQUIREMENTS OF THE CONSTRUCTION SAFETY AND PHASING PLAN WILL BE MET. THE SPCD SHALL INCLUDE A CERTIFICATION STATEMENT BY THE CONTRACTOR THAT:

1. INDICATES FULL UNDERSTANDING OF THE OPERATIONAL SAFETY REQUIREMENTS OF THE CONSTRUCTION SAFETY AND PHASING PLAN AND;
2. ASSERTS NO DEVIATIONS SHALL BE MADE FROM THE APPROVED SAFETY PHASING PLAN AND SPCD UNLESS WRITTEN APPROVAL IS GRANTED BY THE AIRPORT. THE SPCD SHALL FOLLOW THE FORMAT OF THE MOST CURRENT EDITION OF FAA AC 150/5370-2 AND SHALL ADDRESS ITEMS (1)-(18) PER THE REQUIREMENTS OF THE AC.

B. THE CONTRACTOR SHALL SUBMIT THE SPCD TO THE RESIDENT PROJECT REPRESENTATIVE FOR REVIEW AND APPROVAL A MIN. OF TEN (10) DAYS PRIOR TO THE ANTICIPATED ISSUE OF THE NOTICE TO PROCEED. COPIES OF THE APPROVED CONSTRUCTION SAFETY PHASING PLAN AND SAFETY PLAN COMPLIANCE DOCUMENT SHALL BE MAINTAINED AT THE PROJECT SITE.

C. DAILY INSPECTIONS BY THE CONTRACTOR AND AIRPORT OPERATOR TO ENSURE COMPLIANCE WITH THE CONSTRUCTION SAFETY PHASING PLANS SHALL BE PERFORMED.

D. THE CONTRACTOR MUST IMMEDIATELY NOTIFY AIRPORT PERSONNEL OF ANY CONDITIONS ADVERSELY AFFECTING THE SAFE OPERATION ON THE AIRFIELD. CONTACT INFORMATION WILL BE PROVIDED AT THE PRE-CONSTRUCTION MEETING.

RESTRICTED AREAS / SAFETY AREAS

A. ALL CONSTRUCTION ACTIVITY SHALL BE COORDINATED THROUGH THE RESIDENT PROJECT REPRESENTATIVE OR ENGINEER AND AIRPORT. CONSTRUCTION ACTIVITY WILL BE AUTHORIZED AFTER NOTAMS HAVE BEEN ISSUED BY THE AIRPORT. AFTER BARRICADES AND LIGHTING PROVISIONS HAVE BEEN IMPLEMENTED BY THE CONTRACTOR, AND AFTER IT HAS BEEN DETERMINED THAT THE HEIGHT OF EQUIPMENT AND MATERIALS ARE BEYOND THE REACH OF THE AIRCRAFT, CONTRACTOR IS RESPONSIBLE TO SUBMIT TO THE FAA A SEPARATE 7460 FOR CONSTRUCTION EQUIPMENT OVER 15 FEET HIGH, AND EXPECT A 45 WORKDAY REVIEW PERIOD.

B. STOCKPILED MATERIALS AND EQUIPMENT STORAGE ARE NOT PERMITTED WITHIN THE SAFETY AREA OR OBJECT FREE AREA OF AN OPERATIONAL RUNWAY OR TAXIWAY. IF THE CONTRACTOR WISHES TO STOCKPILE MATERIALS OR EQUIPMENT ADJACENT TO A RUNWAY/TAXIWAY OBJECT FREE AREA, HE MUST FIRST COORDINATE WITH THE AIRPORT OPERATOR TO ENSURE THAT

1. APPROPRIATE LIGHTING AND BARRICADES ARE IN PLACE, AND
2. THE STOCKPILED MATERIALS DO NOT CREATE A WILDLIFE ATTRACTANT OR FOREIGN OBJECT DEBRIS (FOD) HAZARD. EXCAVATION AND RESULTANT STOCKPILES ADJACENT TO OTHER PAVED SURFACES MUST BE APPROPRIATELY MARKED WITH BARRICADES, AS DIRECTED BY THE AIRPORT.

C. DEBRIS SHALL NOT BE DEPOSITED ON ANY PORTION OF AN OPERATIONAL RUNWAY, TAXIWAY OR APRON. SHOULD ANY DEBRIS BE DEPOSITED ON ACTIVE OPERATIONAL AREAS, EITHER INTENTIONALLY OR ACCIDENTALLY, IT SHALL BE REMOVED IMMEDIATELY TO THE SATISFACTION OF THE RPR AND THE AIRPORT.

D. CLOSING OF RUNWAYS OR TAXIWAYS: WHEN IT BECOMES NECESSARY TO CLOSE OR CHANGE THE STANDARD OPERATIONS OF A RUNWAY OR TAXIWAY, THE CONTRACTOR SHALL, THROUGH THE RPR, NOTIFY THE AIRPORT IN WRITING ONE WEEK IN ADVANCE OF THE PROPOSED CHANGE IN OPERATIONS. SHOULD A RUNWAY OR TAXIWAY BE TEMPORARILY CLOSED, THE CONTRACTOR SHALL COORDINATE WITH AIRPORT OPERATIONS. WHEN CLOSING THE RUNWAY, THE CONTRACTOR SHALL COORDINATE WITH AIRPORT OPERATIONS AND CONTRACTOR SHALL SUPPLY LIGHTED FLASHING "X" SIGNS. ALL OF THESE SAFETY CONTROL DEVICES SHALL REMAIN IN PLACE UNTIL AIRPORT OPENS AFFECTED RUNWAY OR TAXIWAY TO AIRCRAFT OPERATIONS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN THE SAFETY CONTROL DEVICES THROUGHOUT THE DURATION OF THE CLOSURE.

E. OPEN-FIRE WELDING OR TORCH CUTTING OPERATIONS ARE PROHIBITED UNLESS ADEQUATE FIRE AND SAFETY PRECAUTIONS ARE PROVIDED AS APPROVED BY THE AIRPORT. USE OF ELECTRICAL BLASTING CAPS IS PROHIBITED ON OR WITHIN 1,000 FEET OF THE AIRPORT PROPERTY. FLARE POTS ARE PROHIBITED WITHIN THE AIR OPERATIONS AREA (AOA).

F. THE CONTRACTOR'S EQUIPMENT IS STRICTLY LIMITED TO THE CONSTRUCTION AREAS DURING CONSTRUCTION AND TO THE CONTRACTOR STAGING/STORAGE AREA DURING NON-CONSTRUCTION PERIODS, EXCEPT AS PROVIDED ON THE PLANS, IN THE CONTRACT SPECIAL PROVISIONS, OR AS MAY BE AUTHORIZED BY THE AIRPORT IN WRITING. MATERIALS SHALL BE STORED IN THE STAGING AREA.

MARKING, LIGHTING, AND BARRICADES

A. NO CONSTRUCTION SHALL BEGIN UNTIL ALL BARRICADES, CLOSURE CROSSES, AND SAFETY MEASURES HAVE BEEN PLACED AS SHOWN ON THE CONSTRUCTION SAFETY AND PHASING PLAN SHEETS AND APPROVED BY THE RPR AND/OR THE AIRPORT PERSONNEL. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PLACING PORTABLE BARRICADES AND CLOSURE MARKERS AS SHOWN TO KEEP VEHICLES FROM ENTERING ACTIVE AREAS AND TO KEEP AIRCRAFT FROM TAXIING INTO AREAS UNDER CONSTRUCTION.

B. ALL CONSTRUCTION EQUIPMENT MUST BE MARKED WITH A 3 FEET BY 3 FEET ORANGE AND WHITE CHECKERED FLAG AND/OR AMBER BEACON. FOR NIGHT CONSTRUCTION, ALL EQUIPMENT MUST BE EQUIPPED WITH AN AMBER BEACON.

C. BARRICADES SHALL BE LOW PROFILE BARRICADES. BARRICADES SHALL BE ADEQUATELY WEIGHTED SO AS TO WITHSTAND WIND, PROPELLER, AND JET BLAST.

D. CONTRACTOR MUST PROVIDE AN EMERGENCY CONTACT AVAILABLE 24 HOURS A DAY, 7 DAYS A WEEK FOR MAINTENANCE OF CONSTRUCTION LIGHTING AND BARRICADED, OR ANY OTHER ISSUES THAT INTERFERE WITH THE OPERATIONS AT THE AIRPORT. CONTRACTOR SHALL, ON A DAILY BASIS, COORDINATE WITH RPR OR AIRPORT TO ENSURE ALL REQUIRED AIRFIELD LIGHTING IS FUNCTIONAL, LIGHTED BARRICADES ARE OPERATIONAL, AND TO REVIEW NOTAMS.

E. TRUCK HAULING SIGNS SHALL BE REMOVED OR COVERED IF TRUCKS ARE NOT HAULING FOR GREATER THAN 24 HOURS AND SHALL BE REINSTALLED OR UNCOVERED ONCE HAULING OPERATIONS RESUME. ALL TEMPORARY TRAFFIC CONTROL SIGNS SHALL BE IN COMPLIANCE WITH MUTCD STANDARDS AND READ AS SHOWN IN THIS PLAN.

F. CONTRACTOR SHALL MAINTAIN BARRICADES DURING WINTER SHUTDOWN.

FUEL SUPPORT

A. ANY TYPE OF FUELING SUPPORT FACILITY OR DEVICE USED TO REFUEL CONSTRUCTION EQUIPMENT IS SUBJECT TO LOCAL FIRE INSPECTION. LOCAL FIRE CODES AND SAFETY STANDARDS SHALL BE MET PRIOR TO COMMENCEMENT OF WORK.

B. LIMIT REFUELING OPERATIONS TO PAVED AREAS.

DUST CONTROL

A. CONTRACTOR SHALL LIMIT THE EMISSION OF PARTICULATE MATTER INTO THE AMBIENT AIR BY PREVENTING, CONTROLLING, AND MITIGATING FUGITIVE DUST FROM ANY AND ALL CONSTRUCTION ACTIVITIES. CONTRACTOR SHALL COMPLY WITH ALL REQUIREMENTS OF THE STATE OF UTAH AND ANY LOCAL DUST ORDINANCES.

B. PRIOR TO ENGAGING IN ANY CONSTRUCTION ACTIVITIES RELATED TO CONSTRUCTION THAT DISTURB OR HAVE THE POTENTIAL TO DISTURB SOILS AND CAUSE FUGITIVE DUST, CONTRACTOR SHALL OBTAIN APPROVAL OF THEIR MITIGATION FROM THE AIRPORT AND ENGINEER.

C. DUST MITIGATION PLAN STIPULATIONS: CONTRACTOR SHALL PROVIDE COMPLETE COPIES OF THE DUST CONTROL PERMIT AND/OR DUST MITIGATION PLAN TO ALL SUBCONTRACTORS UNDER ITS CONTROL AND ASSURE THAT ALL SUCH SUBCONTRACTORS ABIDE BY THE CONDITIONS CONTAINED THEREIN. CONTRACTOR SHALL SUPPLY A COMPLETE COPY OF THE DUST MITIGATION PLAN TO THE ENGINEER.

D. DUST CAN BE VERY DETERIMENTAL TO THE SAFE OPERATION OF AN AIRCRAFT. IF, IN THE OPINION OF AIRPORT STAFF, FUGITIVE DUST IS IN ANY WAY AFFECTING SAFETY OF AIRCRAFT OPERATIONS (TO INCLUDE RESTRICTING VISIBILITY OR INGESTION BY ENGINES), ALL WORK ON THE SITE WILL IMMEDIATELY CEASE UNTIL A SATISFACTORY REMEDY IS DEVELOPED AND IMPLEMENTED. AFTER SUCH A SUSPENSION OF WORK, ONLY A MEMBER OF THE AIRPORT STAFF MAY AUTHORIZE WORK TO RECOMMENCE. ALL COST ASSOCIATED WITH DELAYS IN CONSTRUCTION WITH ANY DELAYS IN REMEDIAL OR CORRECTIVE ACTION, WILL NOT BE REIMBURSED BY THE AIRPORT.

SWEEPING / CLEAN-UP

A. THE CONTRACTOR SHALL HAVE SWEEPING OR VACUUMING CAPABILITIES ON-SITE AT ALL TIMES IN ORDER TO REMOVE DEBRIS FROM ACTIVE OPERATIONAL AREAS (RUNWAYS, TAXIWAYS, APRONS, ENTRANCE ROADS, PARKING LOTS, ETC). MATERIAL TRACKED ONTO THESE AREAS MUST BE CONTINUOUSLY REMOVED DURING THE CONSTRUCTION PROJECT. THE CONTRACTOR MUST CONTINUOUSLY VACUUM OR SWEEP JOINT SEAL MATERIAL AND ASPHALT PIECES AS REMOVAL OCCURS.

B. PRIOR TO OPENING AIRCRAFT MOVEMENT AREAS CLOSED FOR THIS PROJECT, THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE, INCLUDING ADJACENT PAVEMENTS AND HAUL ROUTES, IS CLEAR OF ANY FOREIGN OBJECT DEBRIS (FOD) TO THE SATISFACTION OF THE RPR AND/OR AIRPORT.

C. UPON COMPLETION OF THIS PROJECT, THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE, INCLUDING ADJACENT PAVEMENTS AND HAUL ROUTES, IS RETURNED TO ORIGINAL CONDITION TO THE SATISFACTION OF THE AIRPORT AND RPR.

D. THE AIRPORT AND RPR SHALL INSPECT ALL AREAS FOR COMPLIANCE AND SHALL HAVE THE DISCRETION TO ASK FOR CLEANING IF NEEDED, TO THE

SATISFACTION OF THE AIRPORT AND RPR.

HAUL ROUTES, PLANT SITES AND STAGING AREAS

A. HAUL ROUTES AND ACCESS TO THE CONSTRUCTION SITE(S) WILL BE AS SHOWN ON THE CONSTRUCTION SAFETY AND PHASING PLAN.

B. ALL VEHICLES ARE TO BE PARKED AND SERVICED IN THE CONTRACTOR STAGING/STORAGE AREA. ACCESS TO THE PROJECT SITE SHALL ONLY BE AT DESIGNATED HAUL ROUTE LOCATIONS.

C. CONTRACTOR IS RESPONSIBLE FOR RESTORING ALL HAUL ROADS, PLANT SITES AND STAGING AREAS TO ORIGINAL CONDITION OR BETTER. THIS WILL INCLUDE BUT NOT BE LIMITED TO GRADING (FILLING IF NECESSARY) AND SEEDING ALL TURF AREAS. ANY PAVEMENT AREAS USED BY THE CONTRACTOR AS A HAUL ROUTE WHICH ARE DAMAGED WILL BE REPAVED AND/OR REMARKED AS DIRECTED AND APPROVED BY THE RPR AT THE CONTRACTOR'S EXPENSE.

D. THE CONTRACTOR IS RESPONSIBLE FOR INSPECTING ALL HAUL ROUTES PRIOR TO THE START OF CONSTRUCTION. THE CONDITION OF EXISTING HAUL ROUTES SHALL BE DOCUMENTED EITHER WITH PICTURES OR VIDEO AND PROVIDED TO THE ENGINEER PRIOR TO ANY HAULING ACTIVITIES. ANTICIPATED COSTS ASSOCIATED WITH DOCUMENTING EXISTING HAUL ROUTE CONDITION, OR RECONSTRUCTING OR RESTORING HAUL ROUTES AND STAGING AREAS SHALL BE INCIDENTAL TO THE PROJECT.

AIRPORT DATA TABLE

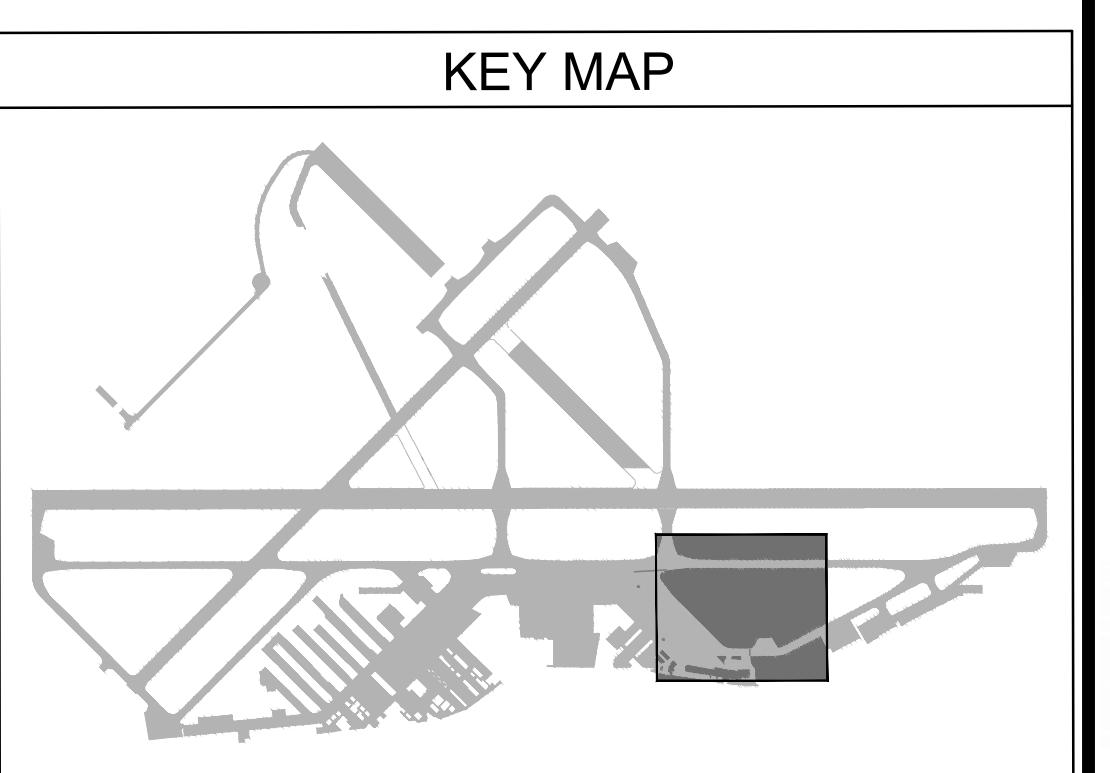
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RUNWAY DESIGN GROUP	C-III	C-II	C-III	-	-
TAXIWAY DESIGN GROUP	-	-	-	TDG 3	TDG 3
RUNWAY SAFETY AREA	500 FEET	500 FEET	500 FEET	-	-
RUNWAY OBJECT FREE AREA	800 FEET	800 FEET	800 FEET	-	-
TAXIWAY SAFETY AREA	-	-	-	118 FEET	118 FEET
TAXIWAY OBJECT FREE AREA	-	-	-	171 FEET	158 FEET

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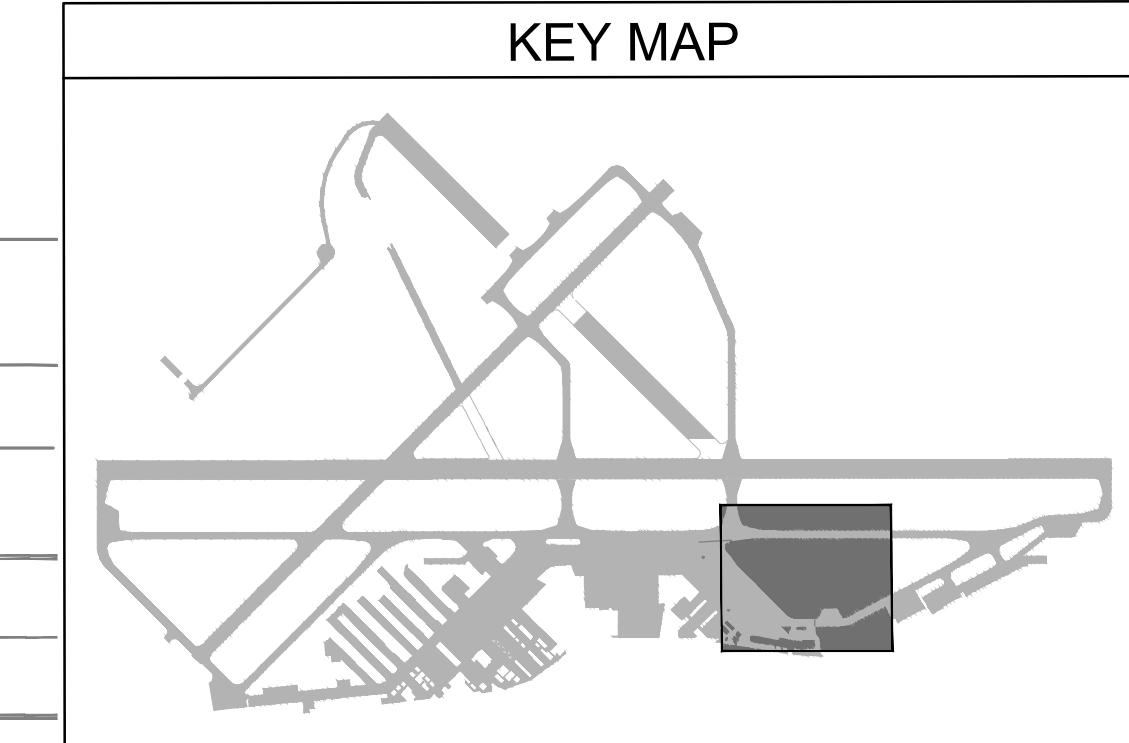
Sheet Contents

EROSION CONTROL PLAN

Sheet No. 14 of 33

C-021

KEY MAP



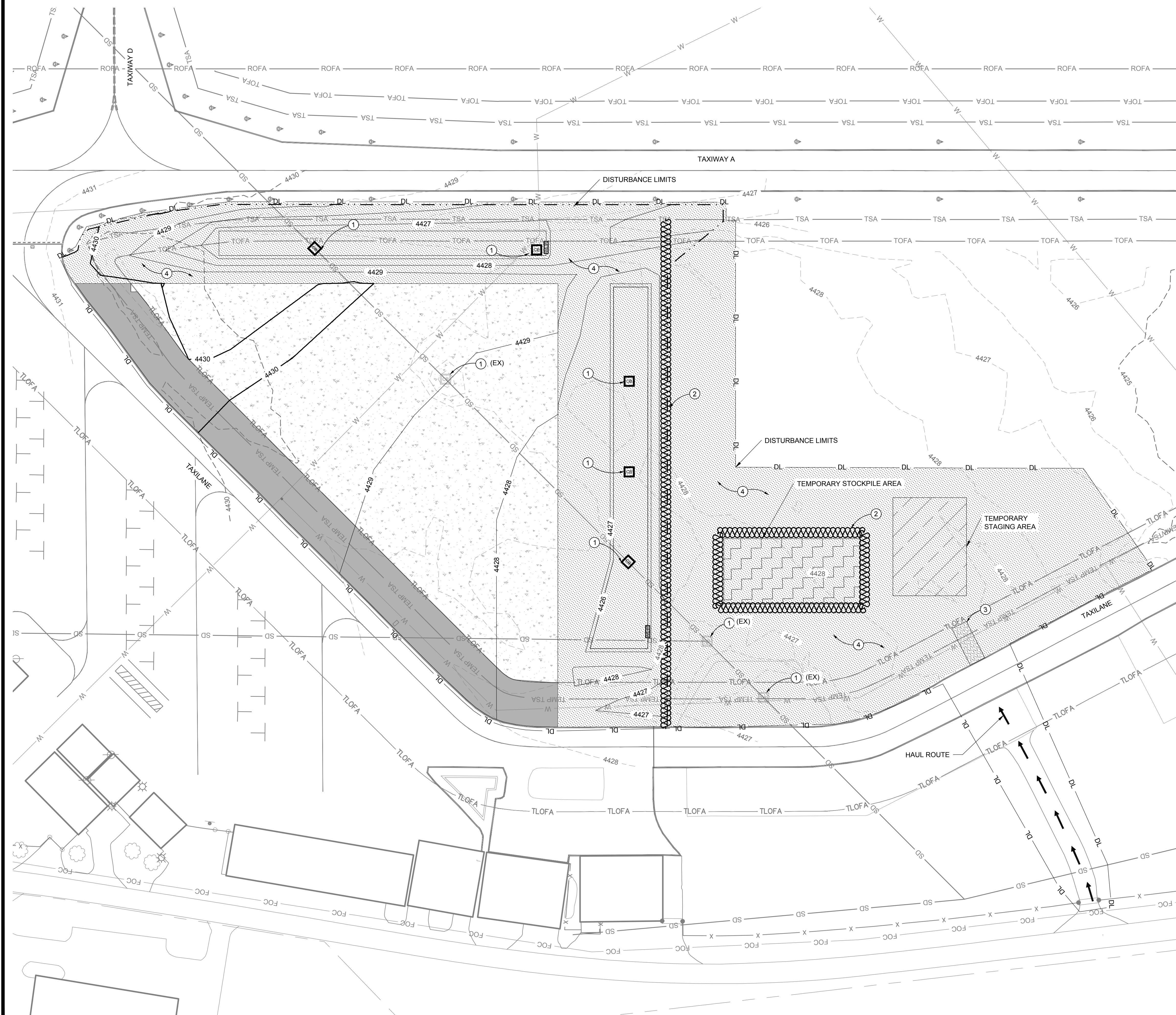
EROSION CONTROL KEY NOTES

- ① CONSTRUCT INLET PROTECTION PER DETAIL 2/C-031
- ② CONSTRUCT SEDIMENT LOGS PER DETAIL 1/C-031
- ③ CONSTRUCT VEHICLE TRACKING CONTROL PER DETAIL 3/C-031
- ④ CONSTRUCT HYDROSEED

LEGEND

- PROPOSED PCC
- PROPOSED AC
- TEMPORARY CONTRACTOR'S STAGING AREA
- TEMPORARY STOCKPILE AREA
- VEHICLE TRACKING CONTROL AREA
- DL DISTURBANCE LIMITS (HYDROSEED LIMITS)
- SEDIMENT LOG
- GRADING LIMIT
- HYDROSEED (MAY - JUNE OR AUGUST - SEPTEMBER)

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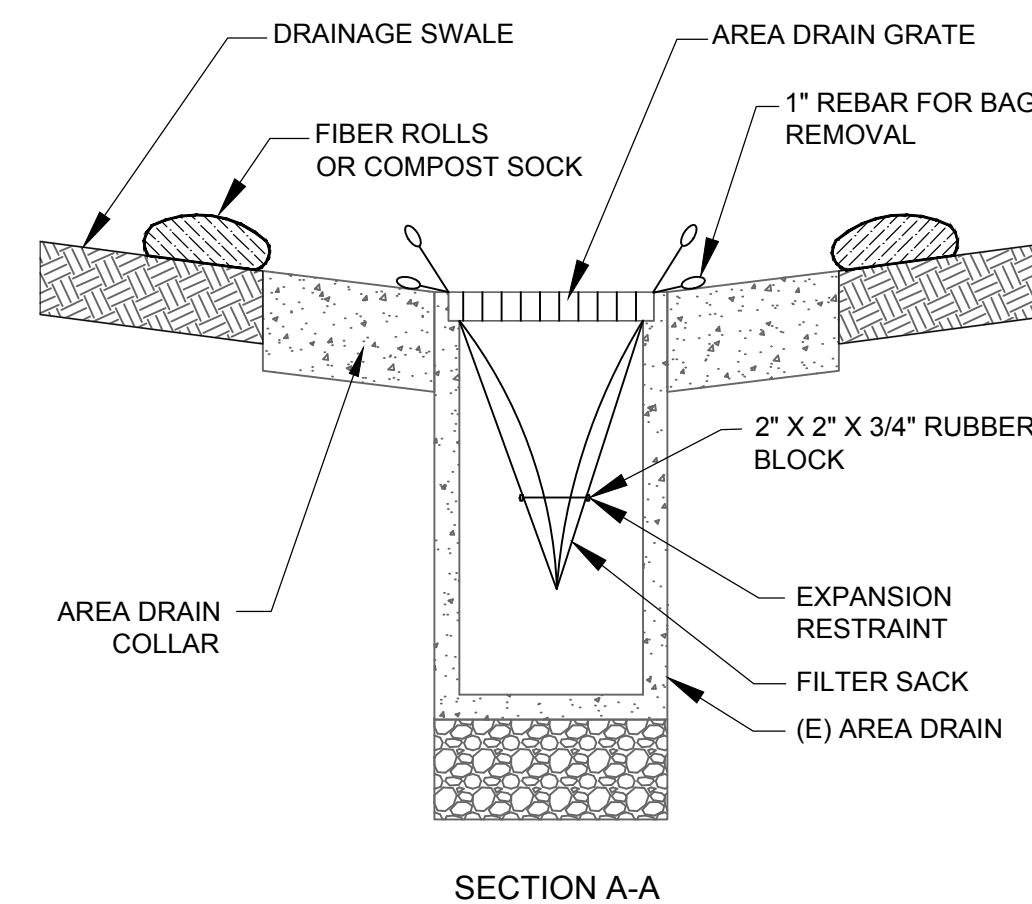
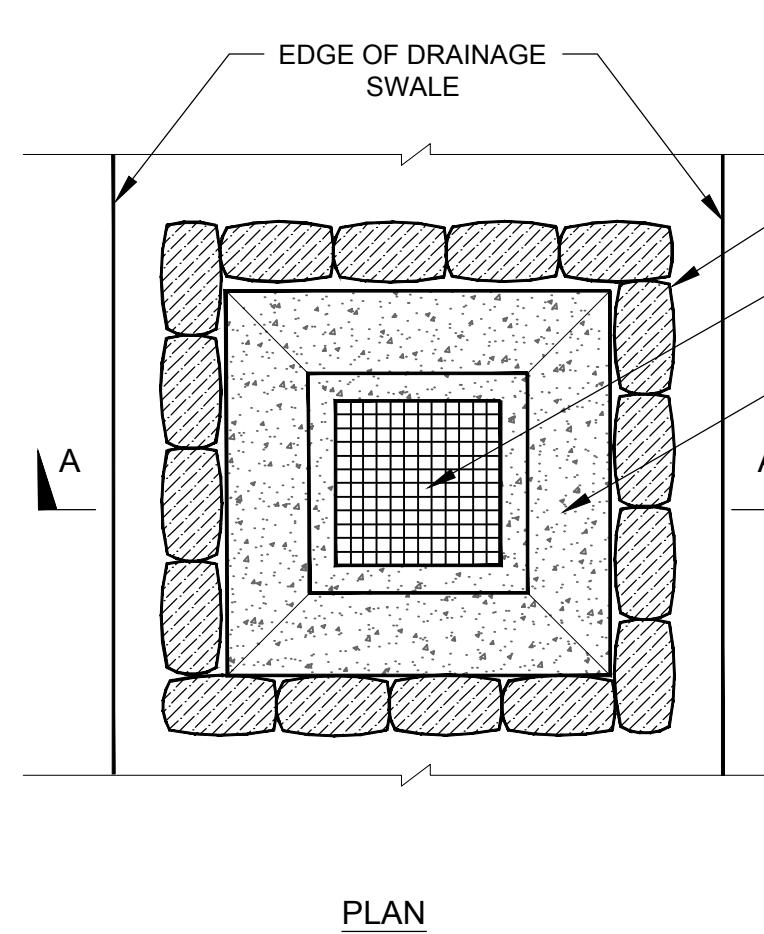




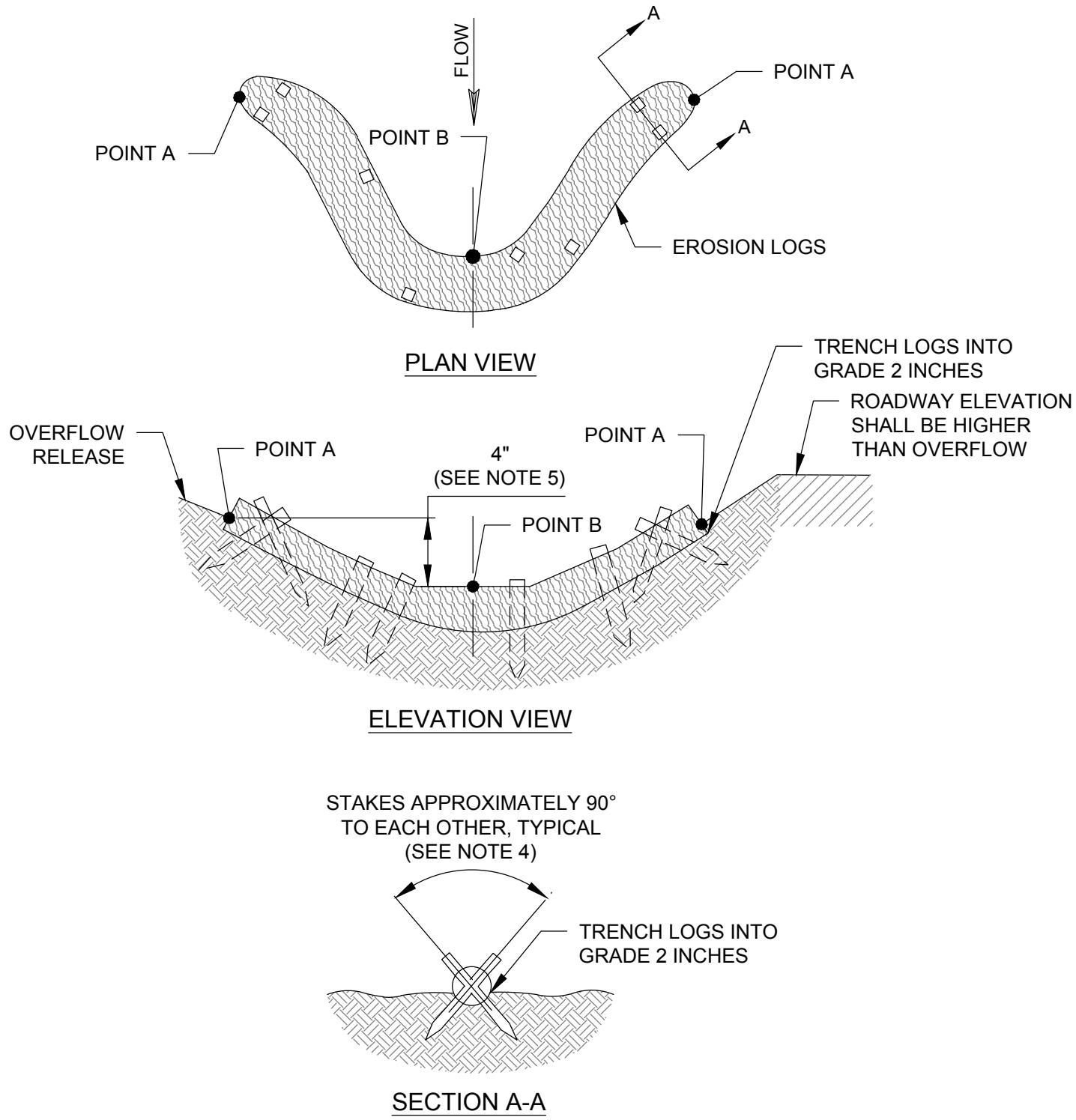
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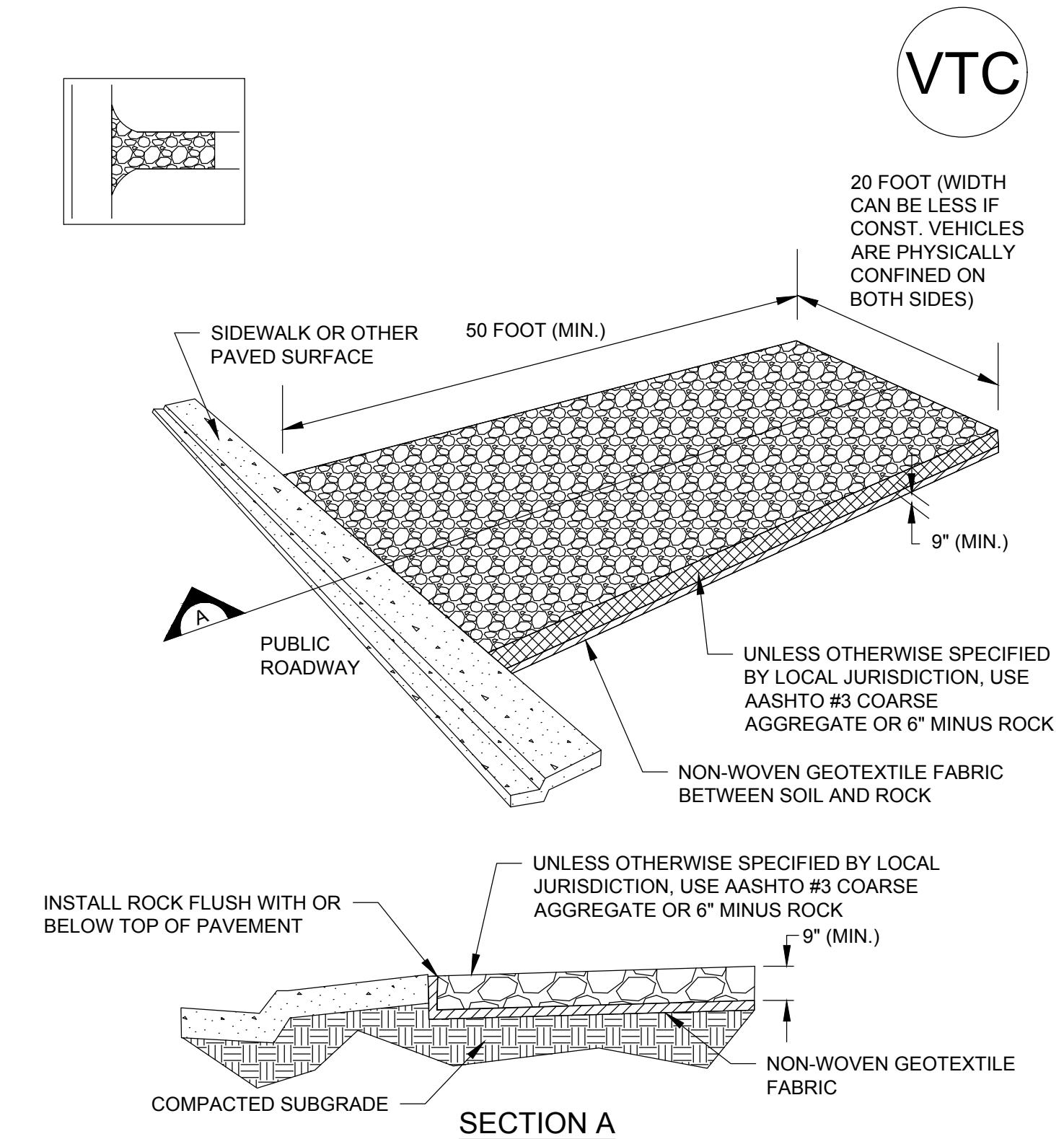
2 INLET PROTECTION NOT TO SCALE



NOTES:

1. EROSION LOGS SHALL BE EMBEDDED 2 INCHES INTO THE SOIL.
2. EROSION LOGS SHALL BE TIGHTLY ABUTTED WITH NO GAPS.
3. V-SHAPED TEMPORARY DITCHES SHALL NOT BE USED. DITCHES SHALL BE GRADED IN THE PARABOLIC OR TRAPEZOIDAL SHAPE.
4. THE TOP OF ALL STAKES SHALL NOT EXTEND MORE THAN 2 INCHES ABOVE THE TOPS OF EROSION LOGS.
5. POINTS "A" SHALL BE A MINIMUM 4 INCHES HIGHER THAN POINT "B".

1 SEDIMENT CONTROL LOG NOT TO SCALE



3 VEHICLE TRACKING CONTROL NOT TO SCALE

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SHEET CONTENTS
EROSION CONTROL DETAILS

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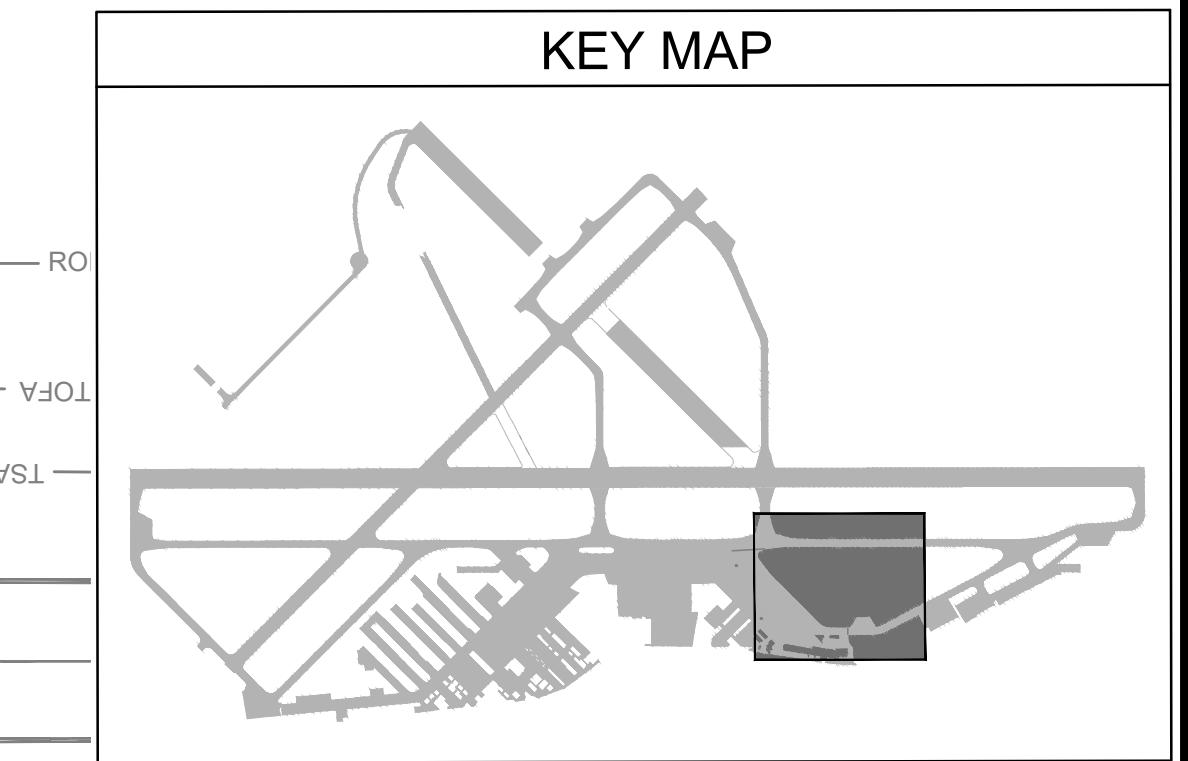
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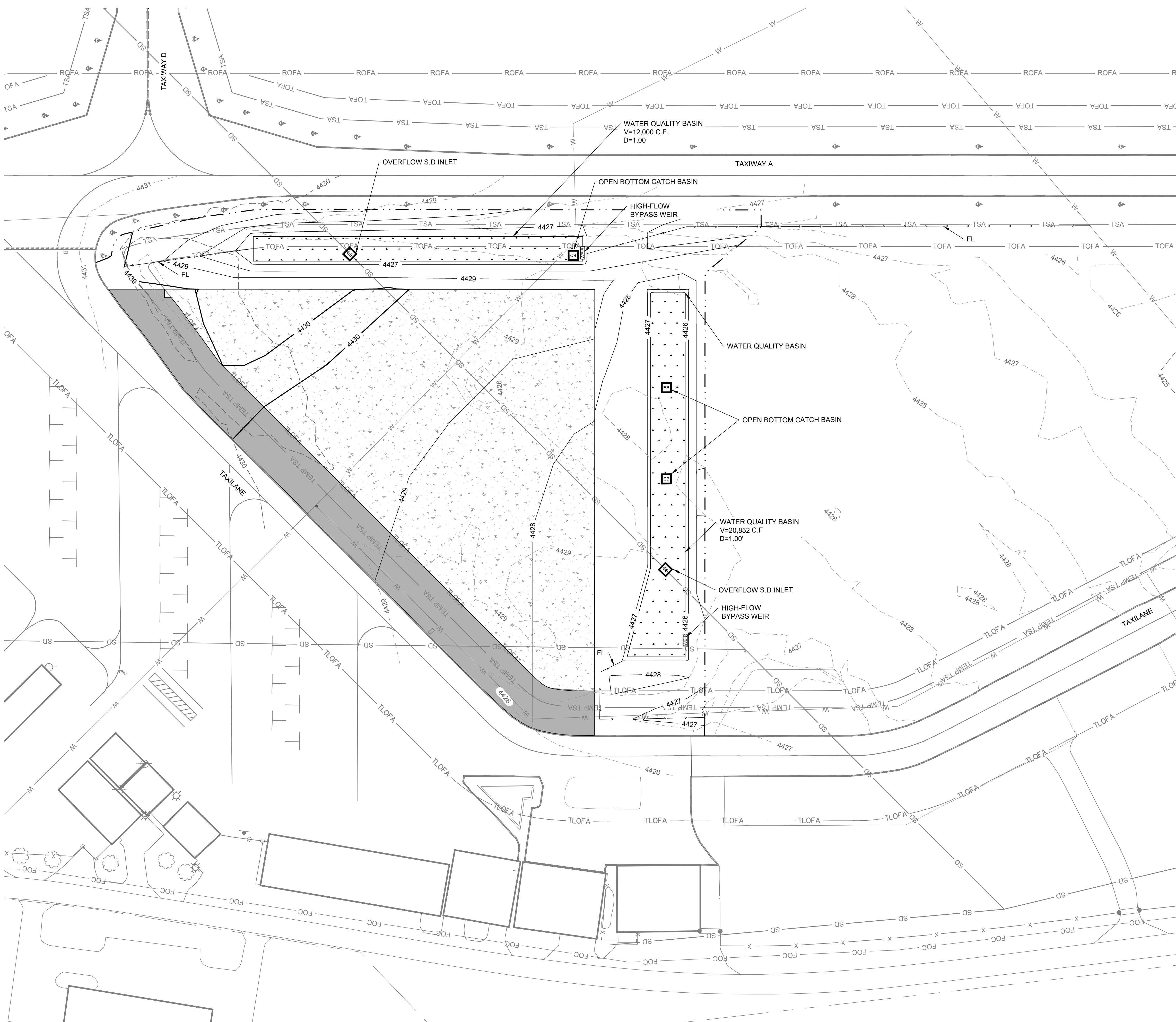
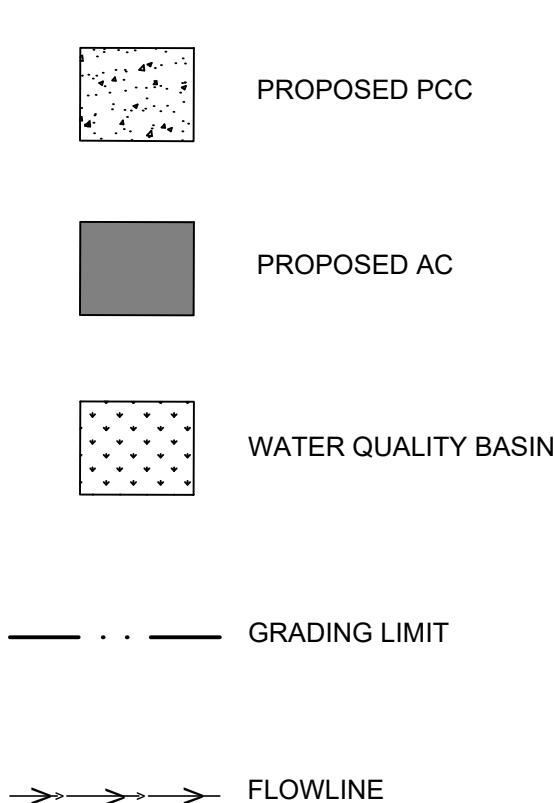
STORM WATER PLAN

SHEET NO. 16 of 33

C-041



KEY MAP





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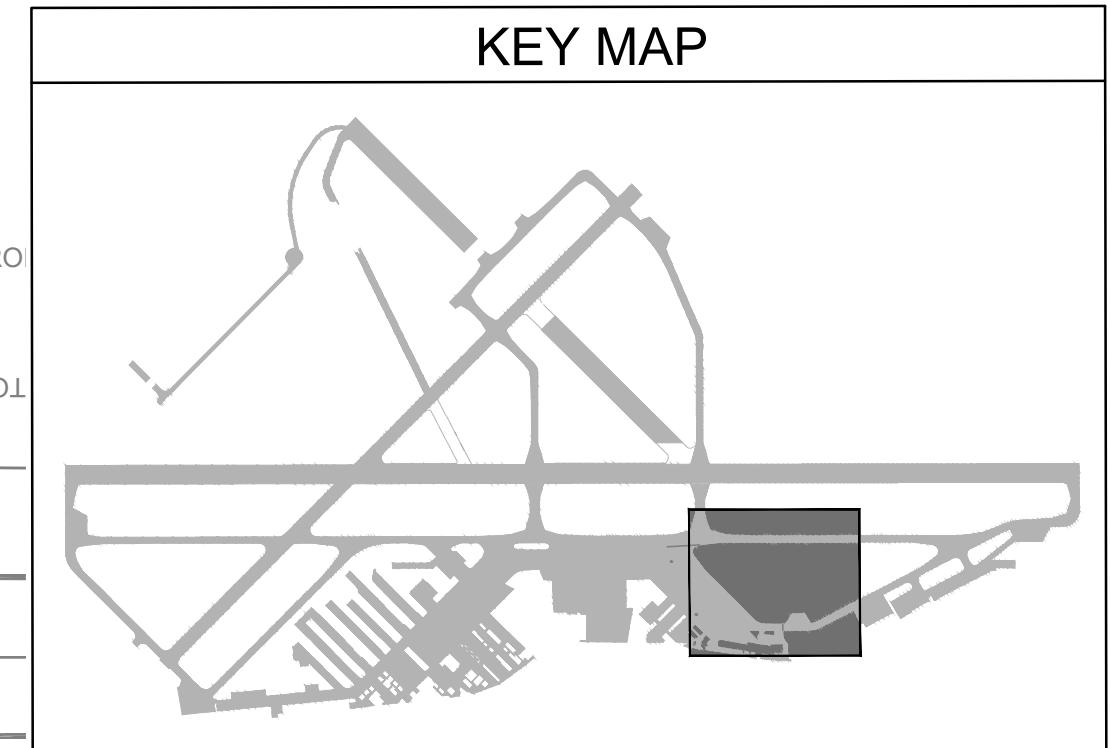
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sheet contents
DEMOLITION PLAN

sheet no.
17 of 33

C-051

KEY MAP

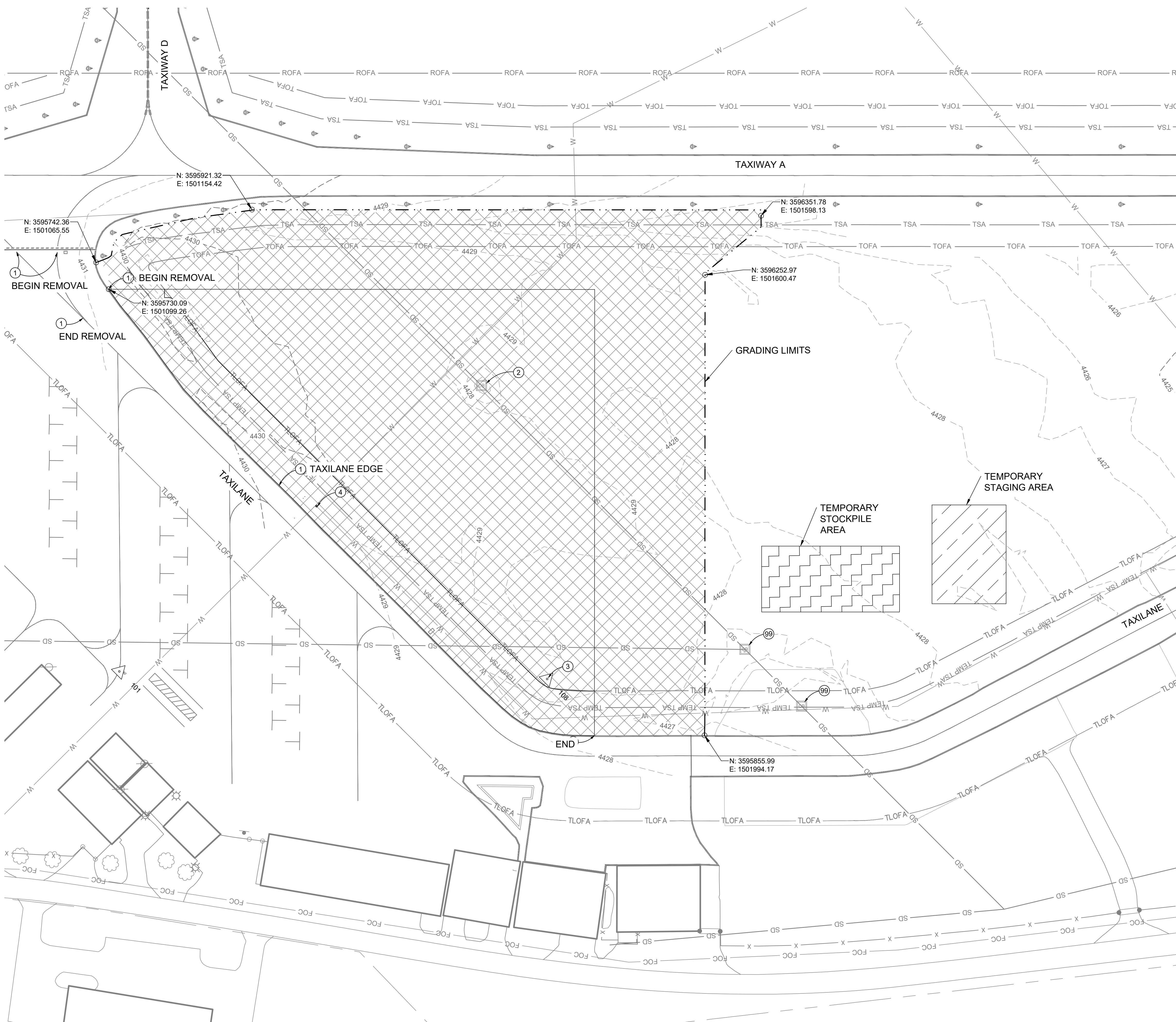


DEMOLITION KEY NOTES

- ① DEMOLISH EXISTING PAVEMENT MARKINGS
- ② DEMOLISH EXISTING STORM DRAIN INLET AND REPLACE WITH RCP PIPE PER DETAIL 1/C-061
- ③ RE-ESTABLISH SURVEY CONTROL POINT, PER CITY OF OGDEN STD. SUR-1. SEE DETAIL 1/C-312
- ④ ADJUST VALVE TO GRADE PER DETAIL 7/C-311
- ⑨99 PROTECT ITEM IN PLACE

LEGEND

- CLEAR AND GRUB
- TEMPORARY CONTRACTOR'S STAGING AREA
- TEMPORARY STOCKPILE AREA
- VEHICLE TRACKING CONTROL AREA
- GRADE LIMIT





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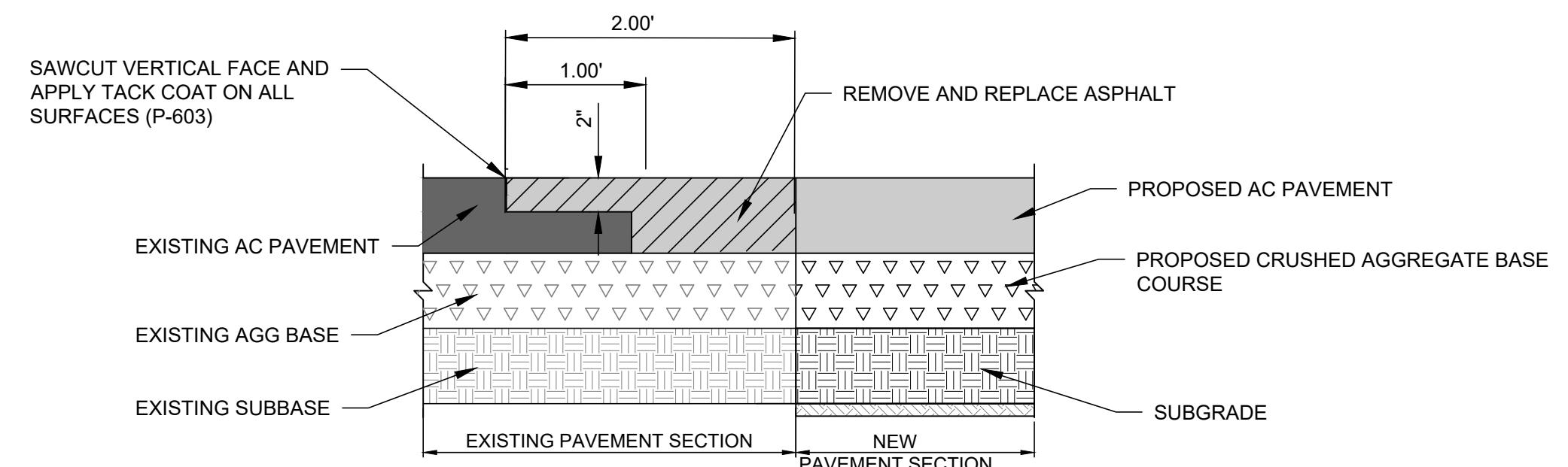
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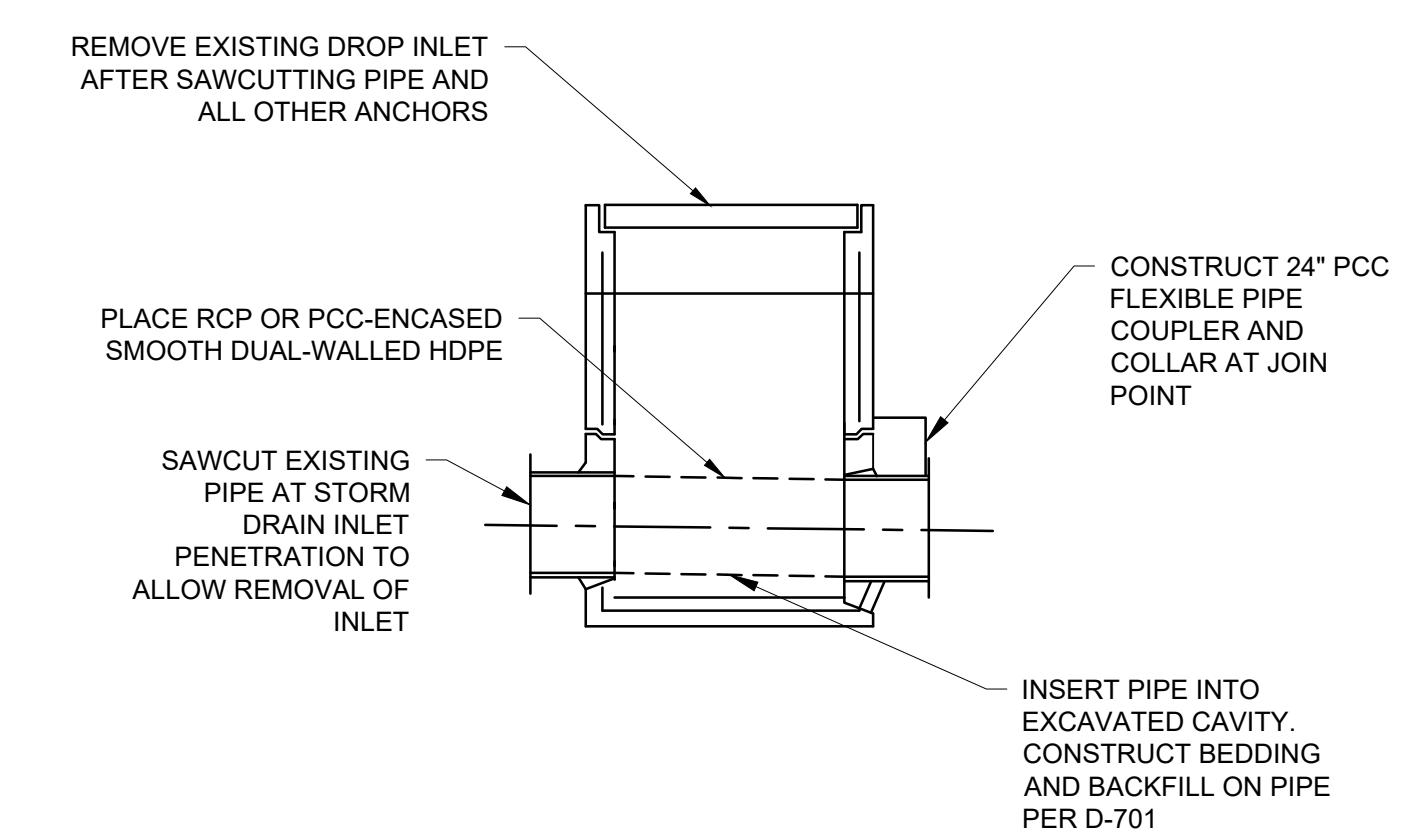
SHEET CONTENTS
DEMOLITION DETAILS

SHEET NO. 18 of 33

C-061



2 TRANSVERSE / LONGITUDINAL PAVEMENT TRANSITION
NOT TO SCALE



1 STORM DRAIN INLET REMOVAL AND PIPE RECONNECT
NOT TO SCALE



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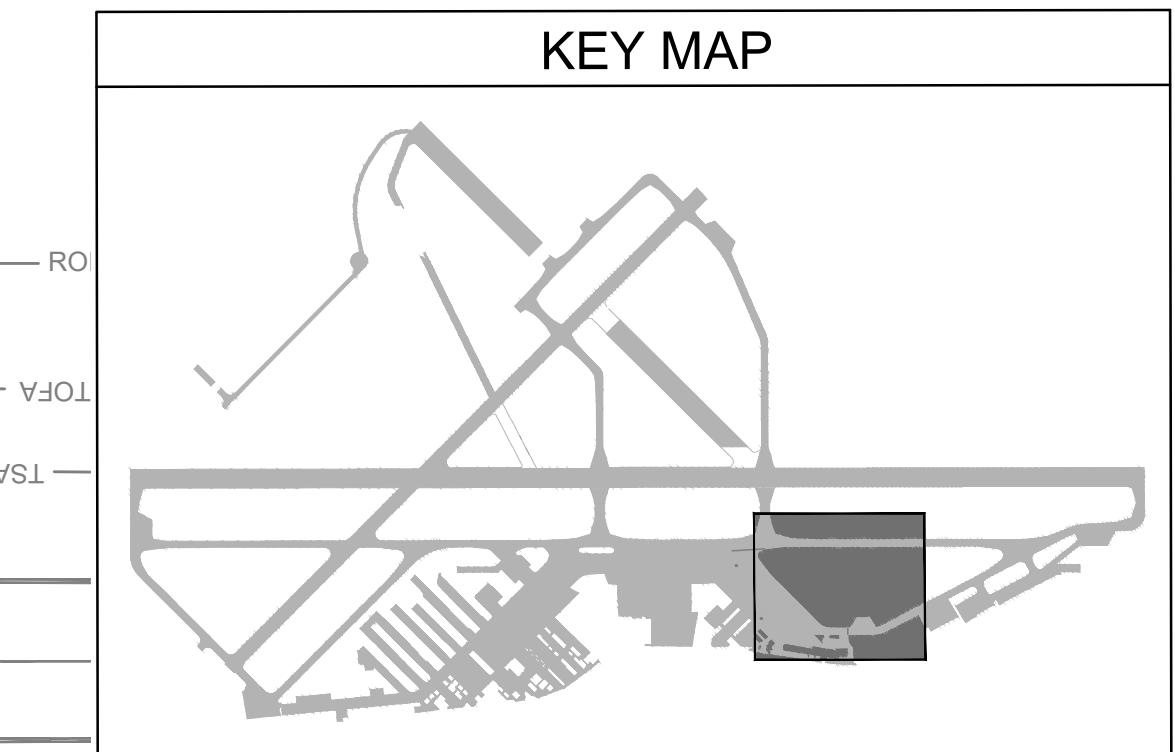
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GEOMETRICS

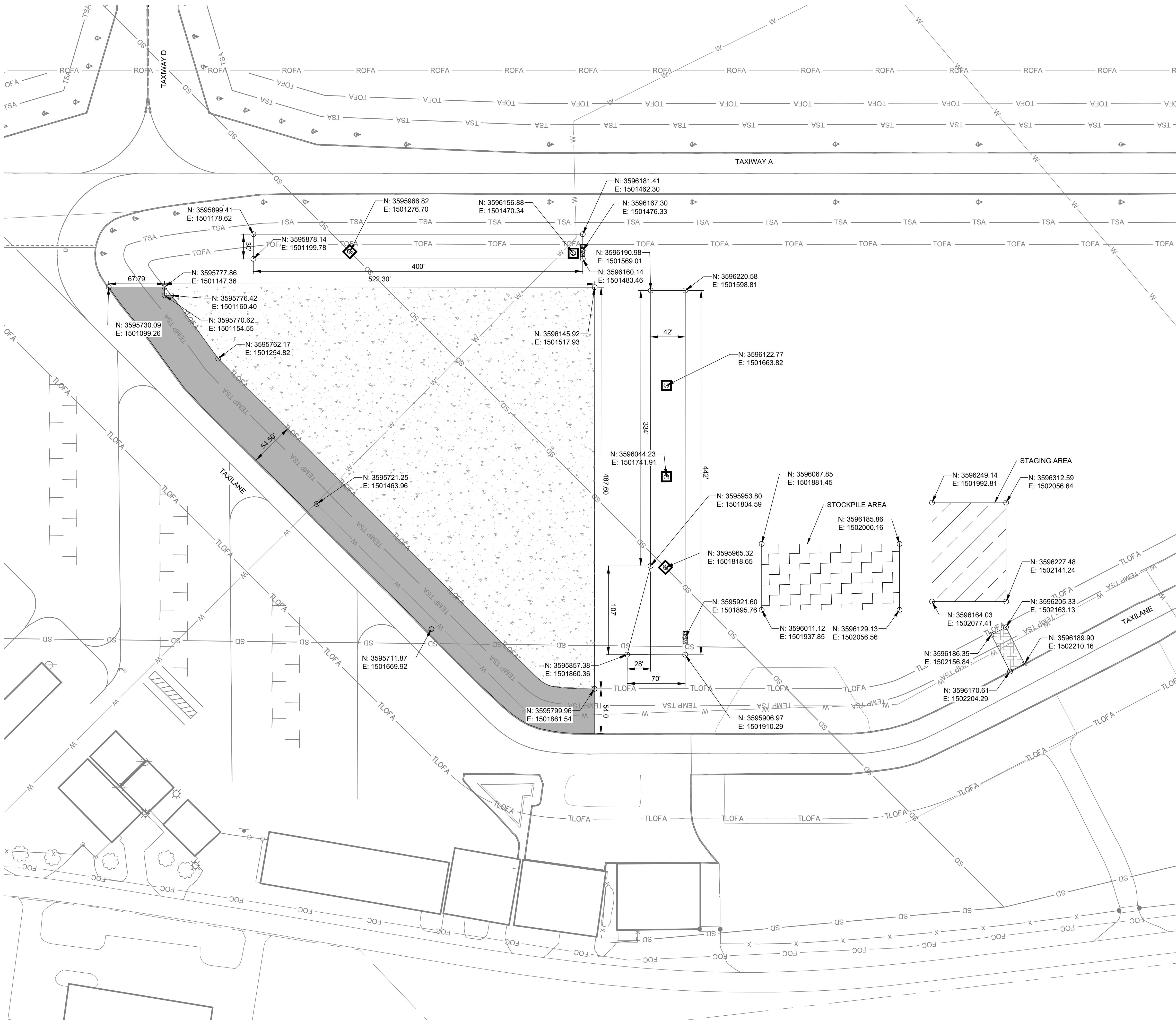
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C-081



LEGEND

- PROPOSED PCC
- PROPOSED AC
- TEMPORARY CONTRACTOR'S STAGING AREA
- TEMPORARY STOCKPILE AREA
- VEHICLE TRACKING CONTROL AREA



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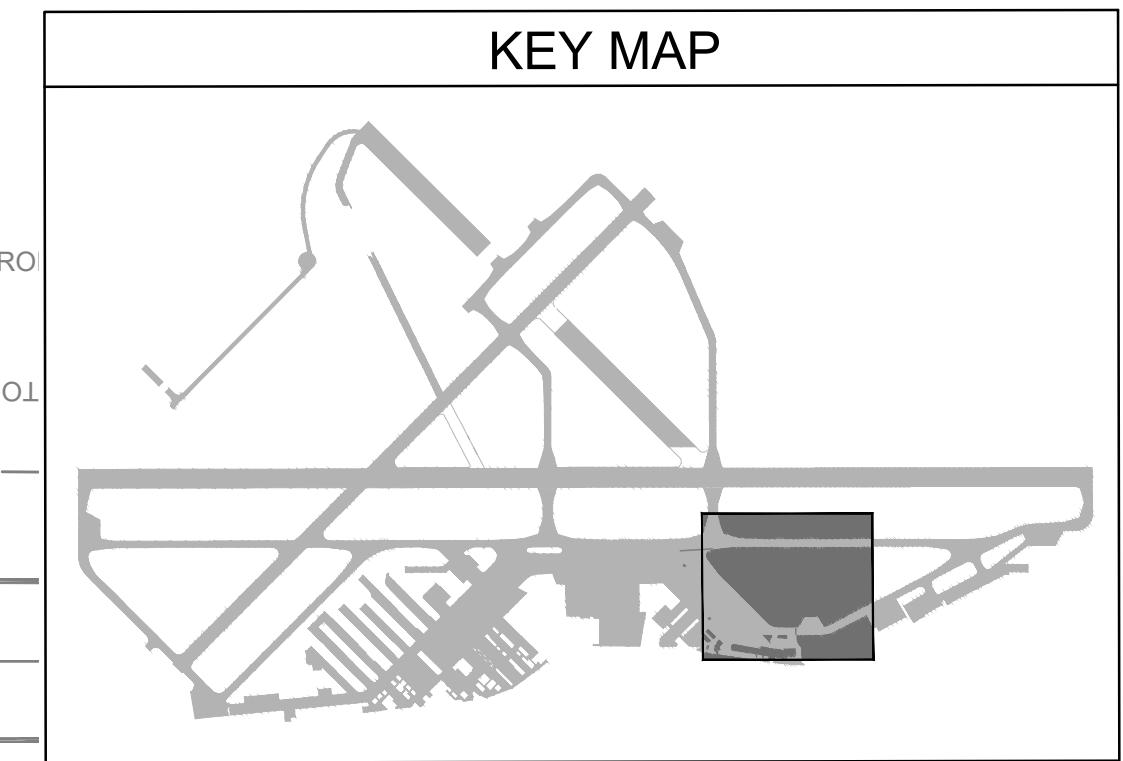
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SHEET CONTENTS

CONSTRUCTION PLAN

SHEET NO. 20 of 33

KEY MAP



CONSTRUCTION KEY NOTES

- ① CONSTRUCT PCC APRON PAVING PER DETAIL 2/C-311
- ② CONSTRUCT AC APRON PER DETAIL 1/C-311
- ③ CONSTRUCT 4'X4' GRATED STORM DRAIN INLET
WITH SPLASH PAD PER DETAIL 3/C-311
- ④ CONSTRUCT OPEN-BOTTOM CATCH BASIN
WITH SPLASH PER DETAIL 4/C-311
- ⑤ CONSTRUCT STORM WATER BASIN PER DETAIL 5/C-311
- ⑥ CONSTRUCT WATER VALVE RISER PER DETAIL 7/C-311
- ⑦ CONSTRUCT ELECTRICAL VAULT MODIFICATION PER 2/C-312
- ⑧ CONSTRUCT BASIN WEIR PER DETAIL 6/C-311
- ⑨ PROTECT IN PLACE

LEGEND

CONSTRUCT PCC

GRADING LIMIT



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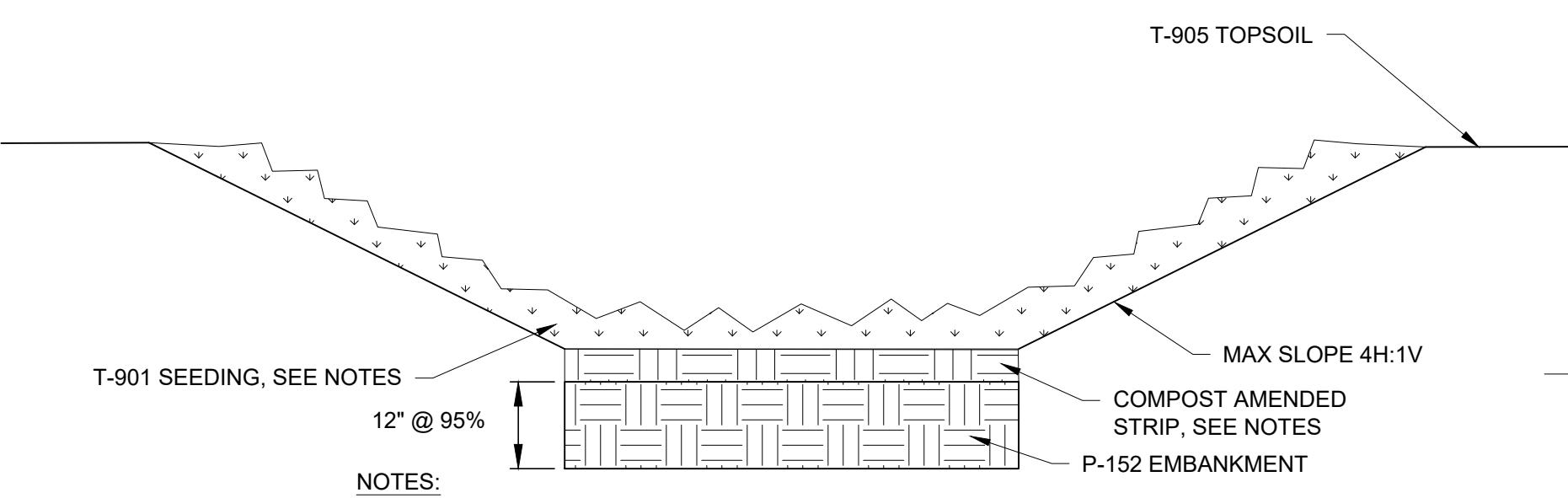
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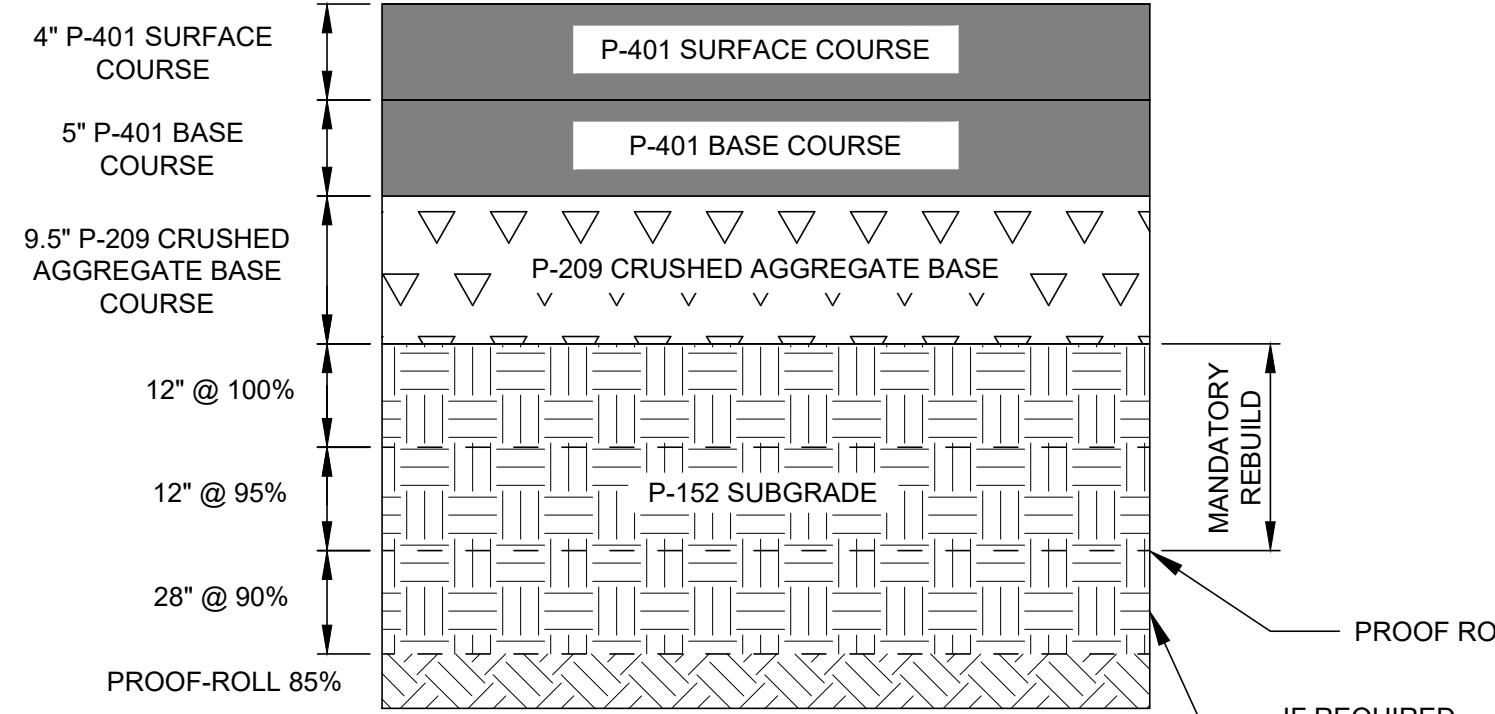
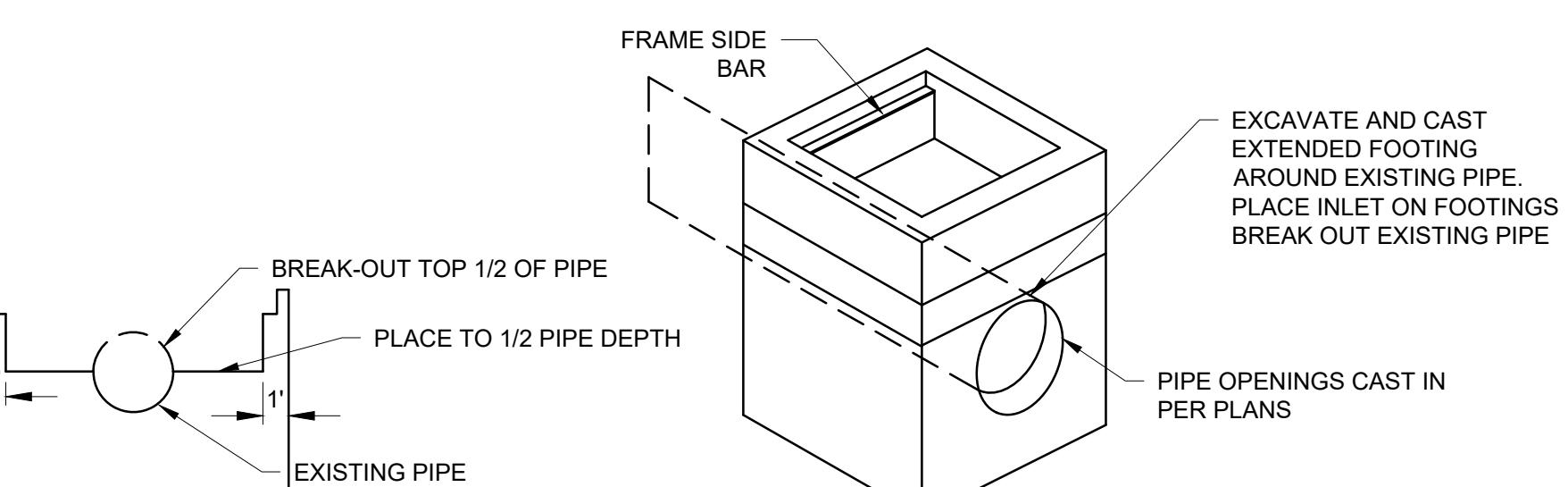
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CONSTRUCTION
DETAILS

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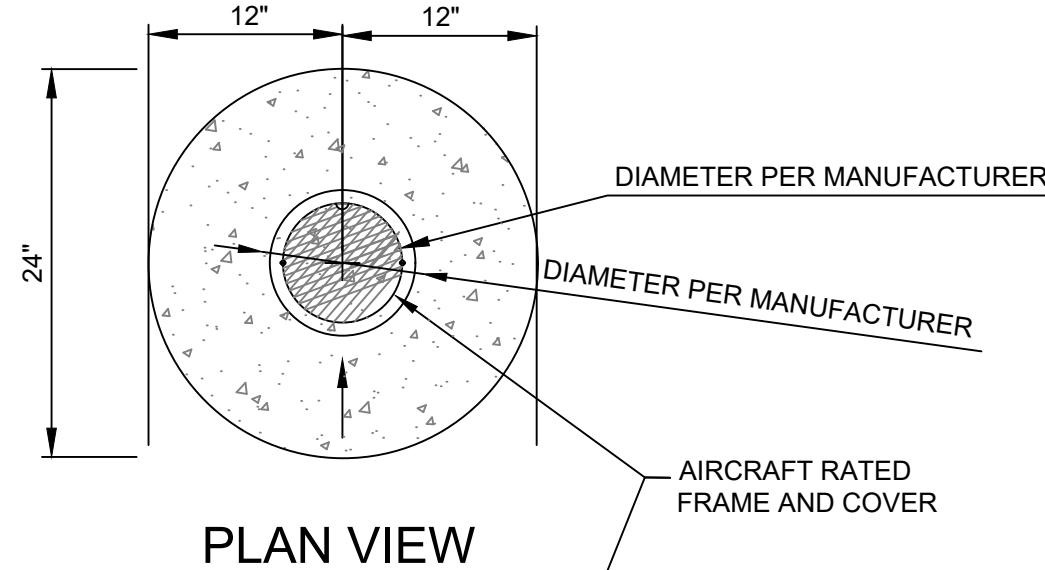
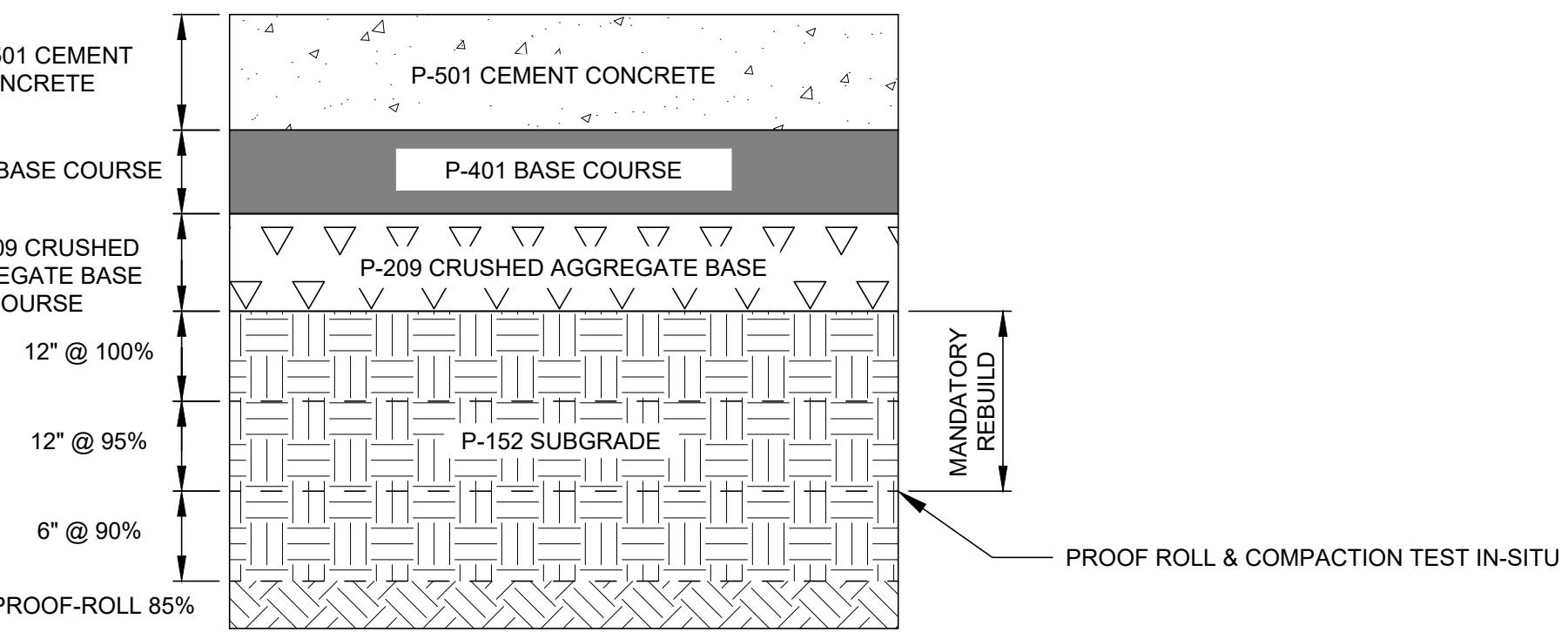
5 BIORETENTION AND GRASS CHANNEL CROSS SECTION



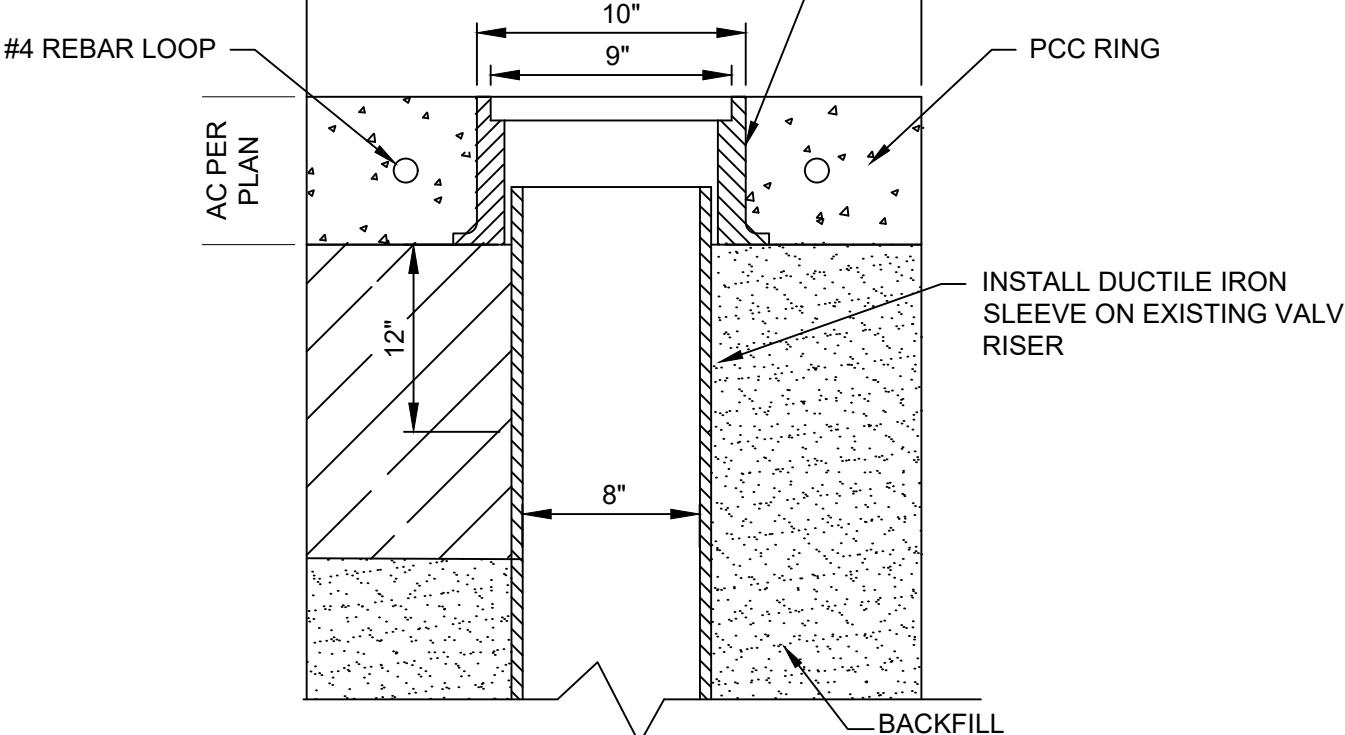
NOTES:

1. COMPOST AMENDED SOIL SHALL EXTENDED OVER THE LENGTH AND WIDTH OF THE CHANNEL BOTTOM.
2. AREAS WHERE A GRASS CHANNEL WILL BE LOCATED SHALL BE READILY IDENTIFIABLE AND PROTECTED FROM UNWANTED ENCROACHMENTS DURING CONSTRUCTION, BOTH ON DEVELOPMENT PLANS AND AT THE CONSTRUCTION SITE. PHYSICAL PROTECTION MEASURES CAN INCLUDE, BUT ARE NOT LIMITED TO, ORANGE FENCING, WOOD OR CHAIN LINK FENCING, AND SIGNAGE.

1 AC PAVEMENT SECTION



PLAN VIEW

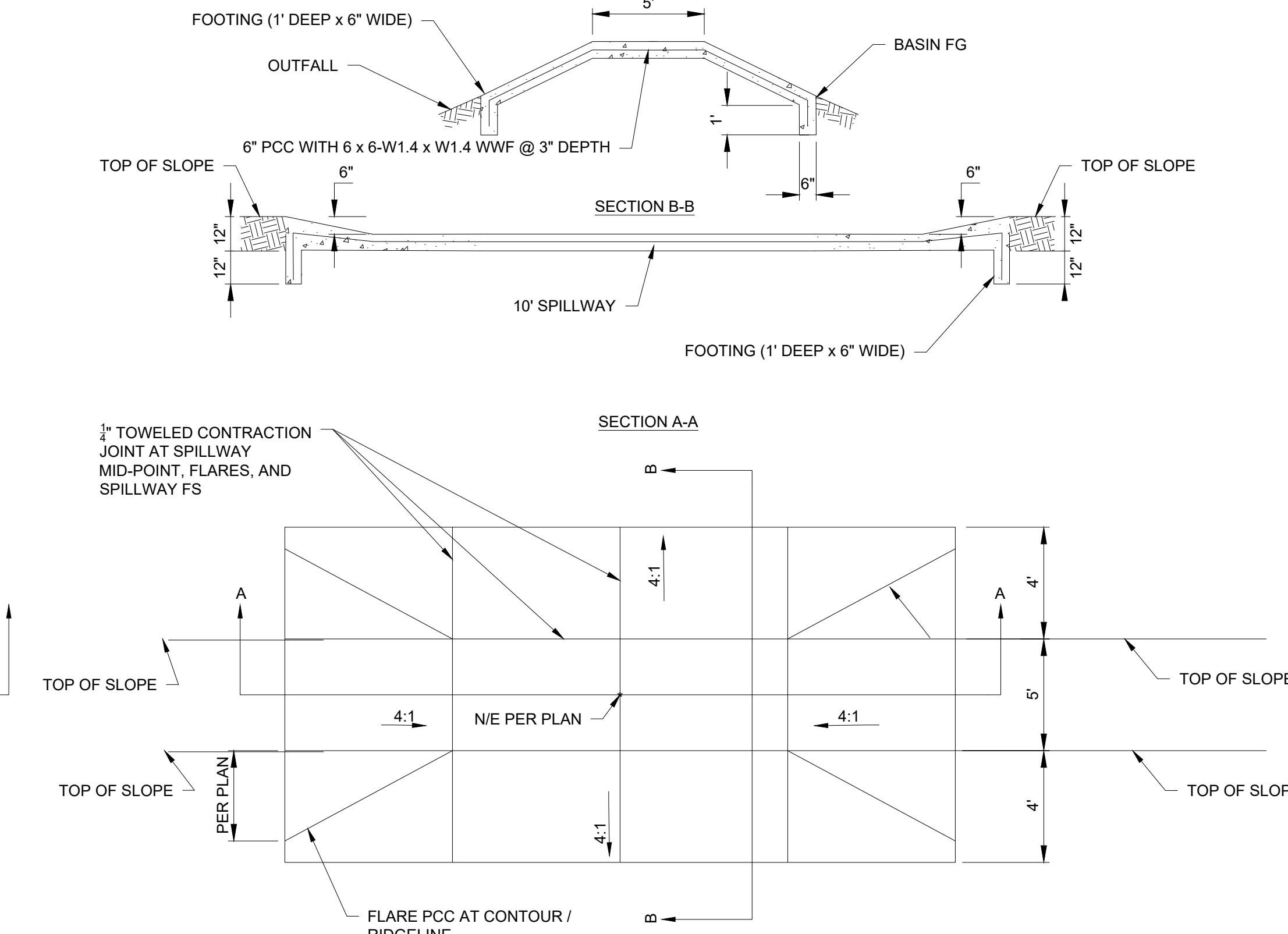


PROFILE VIEW

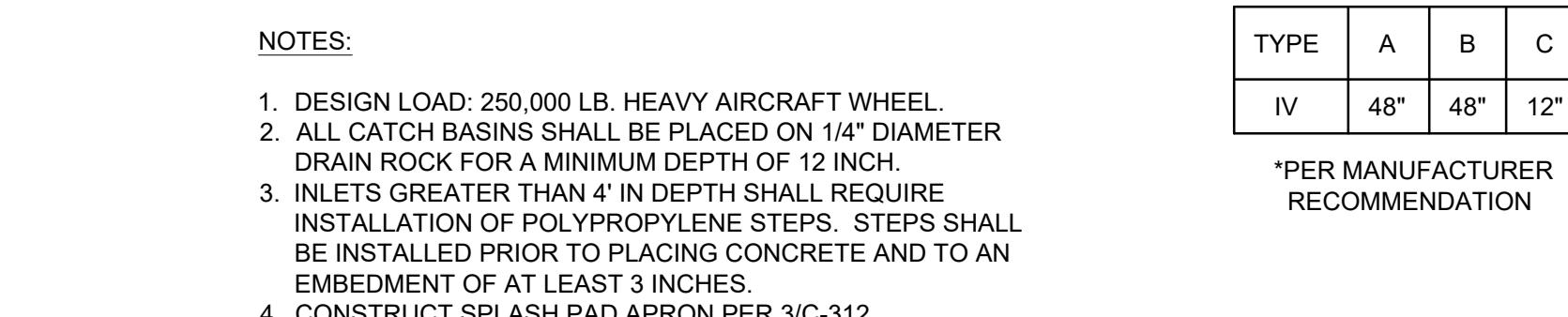
NOTE:

1. STRUCTURES SHALL BE DESIGNED TO SUPPORT 200,000 LB AIRCRAFT WHEEL LOADS WITH 250 PSI TIRE PRESSURE.
2. CASTINGS LIDS SHALL BE BOLTED DOWN. ALL MACHINE THREADED CONNECTIONS SHALL HAVE ANTI-SEIZING COMPOUND.

7 VALVE TO GRADE ADJUSTMENT



6 PCC BASIN WEIR

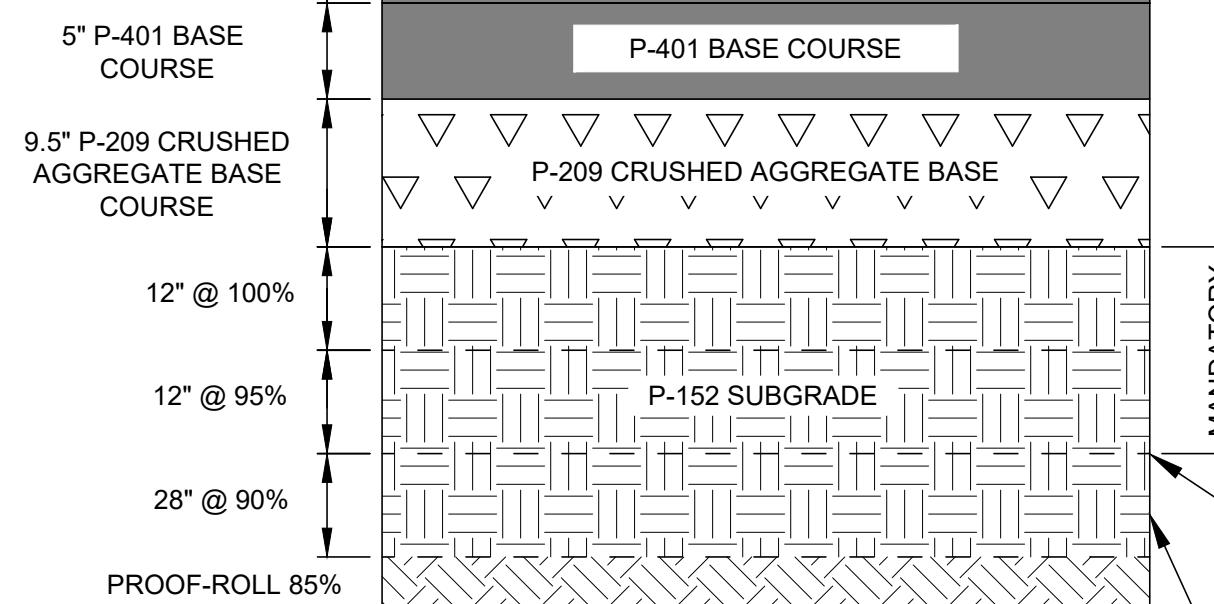


NOTES:

1. DESIGN LOAD: 250,000 LB. HEAVY AIRCRAFT WHEEL.
2. ALL CATCH BASINS SHALL BE PLACED ON 1/4" DIAMETER DRAIN ROCK FOR A MINIMUM DEPTH OF 12 INCH.
3. INLETS GREATER THAN 4" IN DEPTH SHALL REQUIRE INSTALLATION OF POLYPROPYLENE STEPS. STEPS SHALL BE INSTALLED PRIOR TO PLACING CONCRETE AND TO AN EMBEDMENT OF AT LEAST 3 INCHES.
4. CONSTRUCT SPLASH PAD APRON PER 3-C-312.

4 OPEN BOTTOM CATCH BASIN

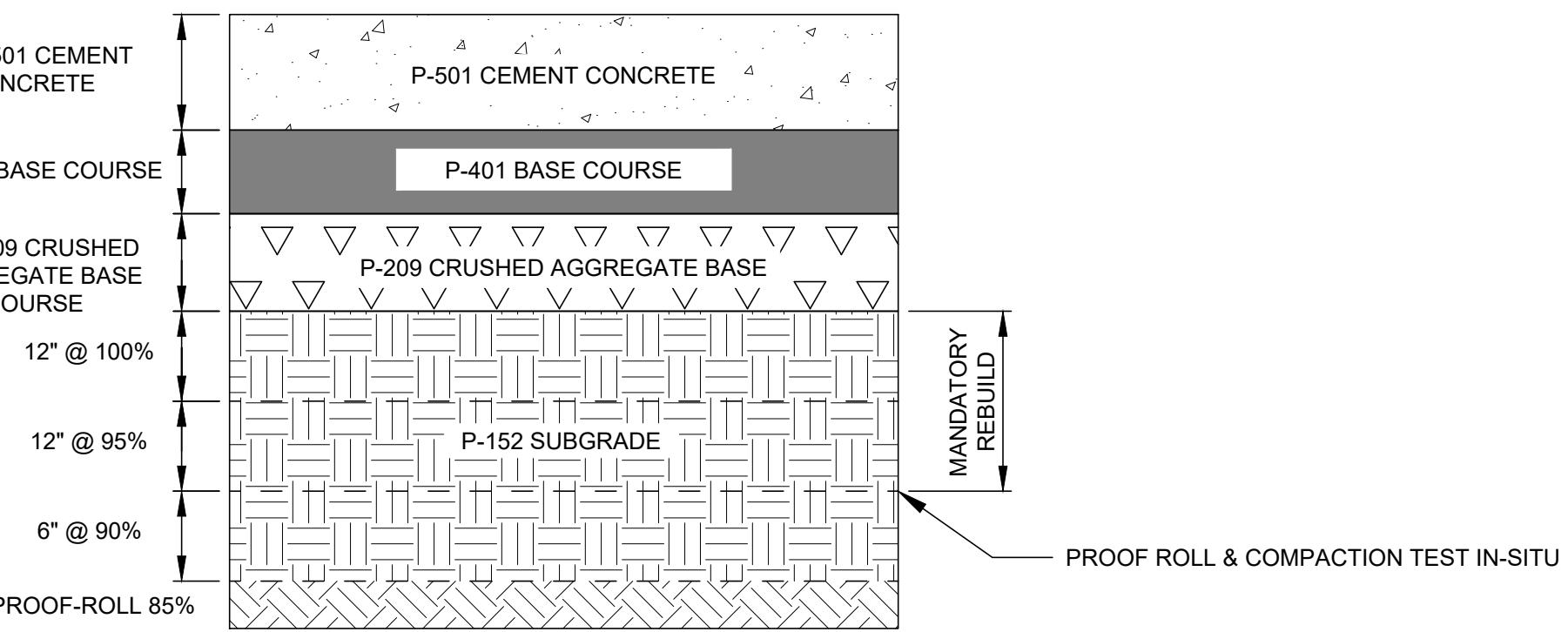
NOT TO SCALE



NOTES:

1. P-603 TACK COAT REQUIRED BETWEEN ALL ASPHALT LIFTS.
2. CONTRACTOR SHALL REBUILD THE 100% AND 95% ZONES. IF IN-SITU SOIL DENSITIES ARE EXCEEDED IN THE 90% ZONE, THAT VOLUME SHALL BE LEFT IN-PLACE.

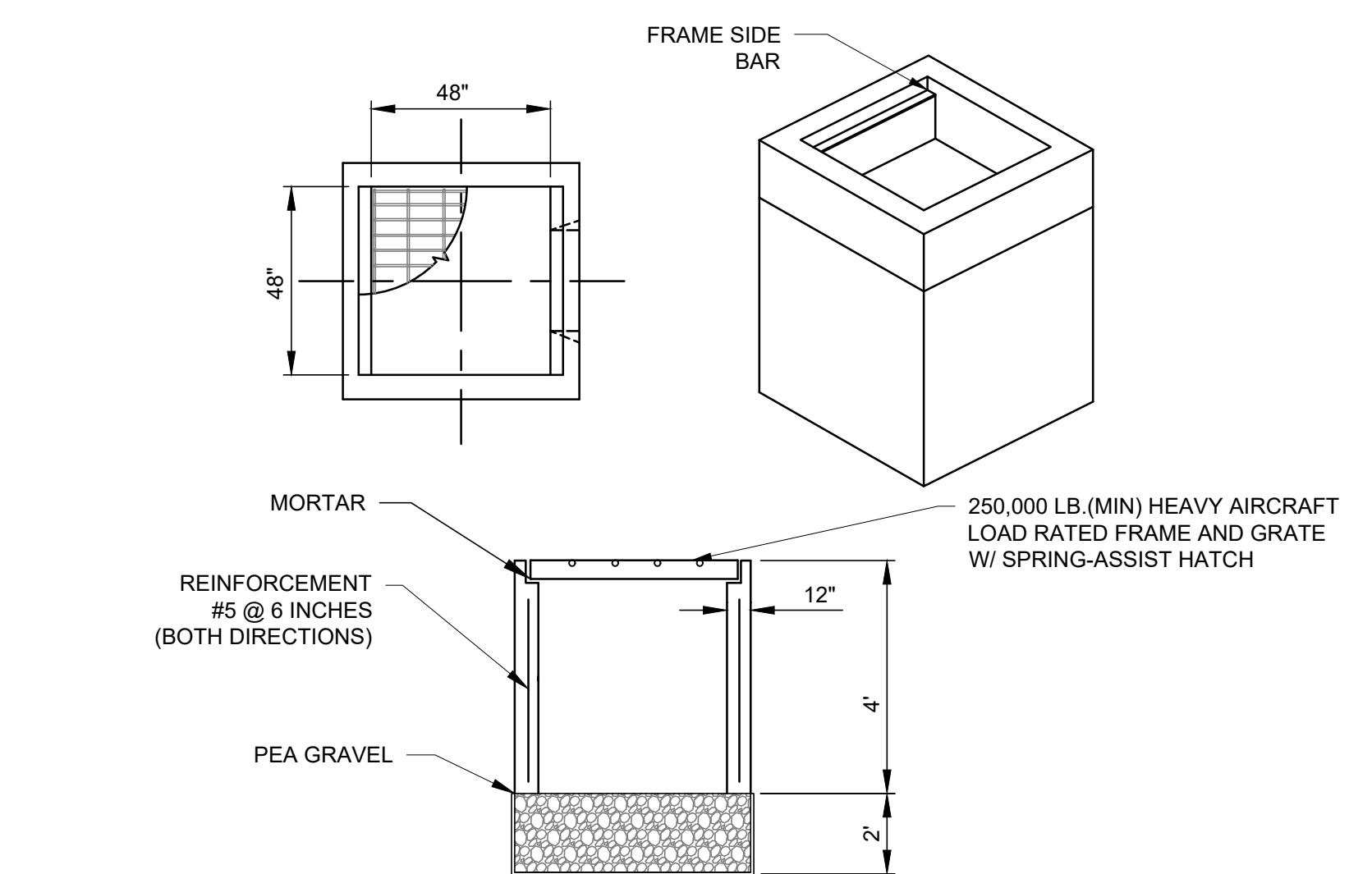
1 AC PAVEMENT SECTION



NOTE:

1. CONTRACTOR SHALL REBUILD THE 100% AND 95% ZONES. IF IN-SITU SOIL DENSITIES ARE EXCEEDED IN THE 90% ZONE, THAT VOLUME SHALL BE LEFT IN-PLACE.

2 PCC PAVEMENT SECTION



NOTES:

1. DESIGN LOAD: 250,000 LB. HEAVY AIRCRAFT WHEEL.
2. ALL CATCH BASINS SHALL BE PLACED ON 1/4" DIAMETER DRAIN ROCK FOR A MINIMUM DEPTH OF 12 INCH.
3. INLETS GREATER THAN 4" IN DEPTH SHALL REQUIRE INSTALLATION OF POLYPROPYLENE STEPS. STEPS SHALL BE INSTALLED PRIOR TO PLACING CONCRETE AND TO AN EMBEDMENT OF AT LEAST 3 INCHES.
4. CONSTRUCT SPLASH PAD APRON PER 3-C-312.

*PER MANUFACTURER RECOMMENDATION

TYPE A B C
IV 48" 48" 12"

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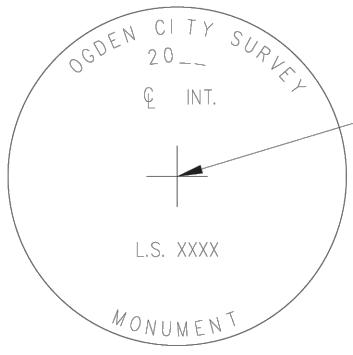
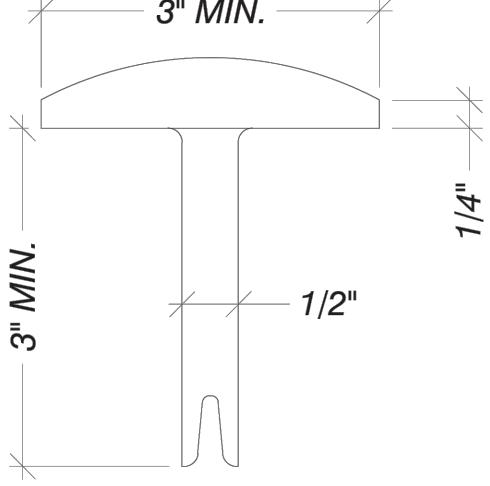
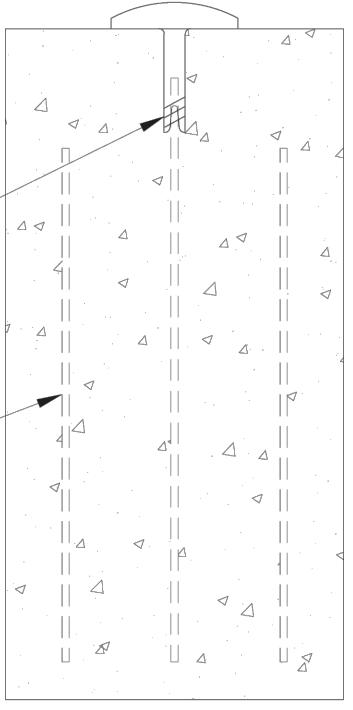
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 <p>CAP PLAN</p> <p>CAP CAN BE PURCHASED FROM ENGINEERS OFFICE</p> <p>CAP SECTION</p> <p>NOTES:</p> <ol style="list-style-type: none"> 1. DATE: SHOW MONTH, DAY, AND YEAR WHEN CAP WAS MARKED. 2. LICENCE: SHOW LICENCE NUMBER OF LAND SURVEYOR WHO MARKED THE CAP. 3. CONCRETE: CLASS 4000 PER APWA SECTION 03 30 04. 4. REINFORCEMENT: ASTM A 615, GRADE 60, DEFORMED STEEL REBAR. 	<p>ABBREVIATIONS:</p> <p>INT - INTERSECTION ML INT - MONUMENT LINE INTERSECTION P.C. - POINT OF CURVATURE P.C.C. - POINT OF COMPOUND CURVATURE P.I. - POINT OF INTERSECTION P.O.C. - POINT ON CURVE P.O.T. - POINT ON TANGENT P.R.C. - POINT OF REVERSE CURVE P.T. - POINT OF TANGENCY S.C. - SECTION CORNER W.C. - WITNESS CORNER</p> <p>SECTION OF BASE (TYPICAL SETTING)</p>			
	 <p>ATTACH CAP TO REBAR WITH WIRE FOR INITIAL PLACEMENT</p> <p>REINFORCEMENT (3) #5 @ 24"</p>			
<p>OGDEN CITY ENGINEERING - STANDARD DRAWINGS</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td style="width: 25%; text-align: center;">  </td> <td style="width: 50%; text-align: center;"> MONUMENT CAP & BASE JUSTIN ANDERSON, CITY ENGINEER </td> <td style="width: 25%; text-align: center;"> SUR-1 SHEET 1 OF 1 </td> </tr> </table>			MONUMENT CAP & BASE JUSTIN ANDERSON, CITY ENGINEER	SUR-1 SHEET 1 OF 1
	MONUMENT CAP & BASE JUSTIN ANDERSON, CITY ENGINEER	SUR-1 SHEET 1 OF 1		

PLAN VIEW

Architectural drawing showing the plan view of a splash pad apron. The apron is 12' wide and 12' long, divided into four 4' x 4' panels. The top two panels are concrete with a 4" thick aggregate base. The bottom two panels are P-610 pavers with a 3" thick aggregate base. A central 4' x 4' area contains a metal grate. Arrows point to various parts of the drawing with the following labels:

- ELEVATION PER PLAN (top left)
- ELEVATION PER PLAN (top right)
- ELEVATION PER PLAN (middle left)
- ELEVATION PER PLAN (middle right)
- ELEVATION PER PLAN (bottom left)
- ELEVATION PER PLAN (bottom right)
- JOINTS PER DETAIL 12/CC-2.2 (TYP)
- 10" P-610 (TYP)
- MATCH EXISTING APRON JOINTS
- TOP OF GRATE (TG) PER PLANS
- FG - 0.05 = 71.95
- FG = 72.0
- 3" CLEAR COVER (TYP)
- 10" (TYP)
- #4 REBAR AT 12" O.C. EACH WAY (TYP)
- P-610
- 3" (TYP)
- EXISTING ASPHALT PAVEMENT
- NOT TO SCALE

SPLASH PAD APRON

NOT TO SCALE

X:\3132200\250253.01\TECH\CAD\SHEETS\c-311 PAVING DETAILS.DWG



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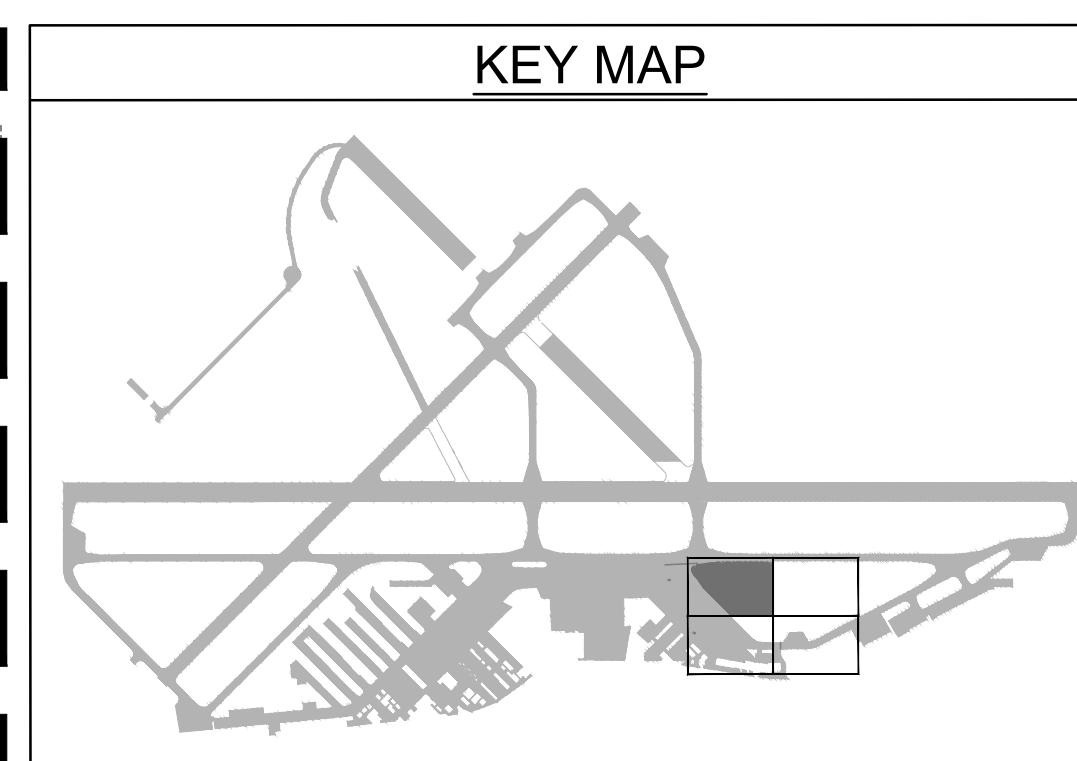
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SHEET CONTENTS
JOINTING PLAN

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KEY MAP

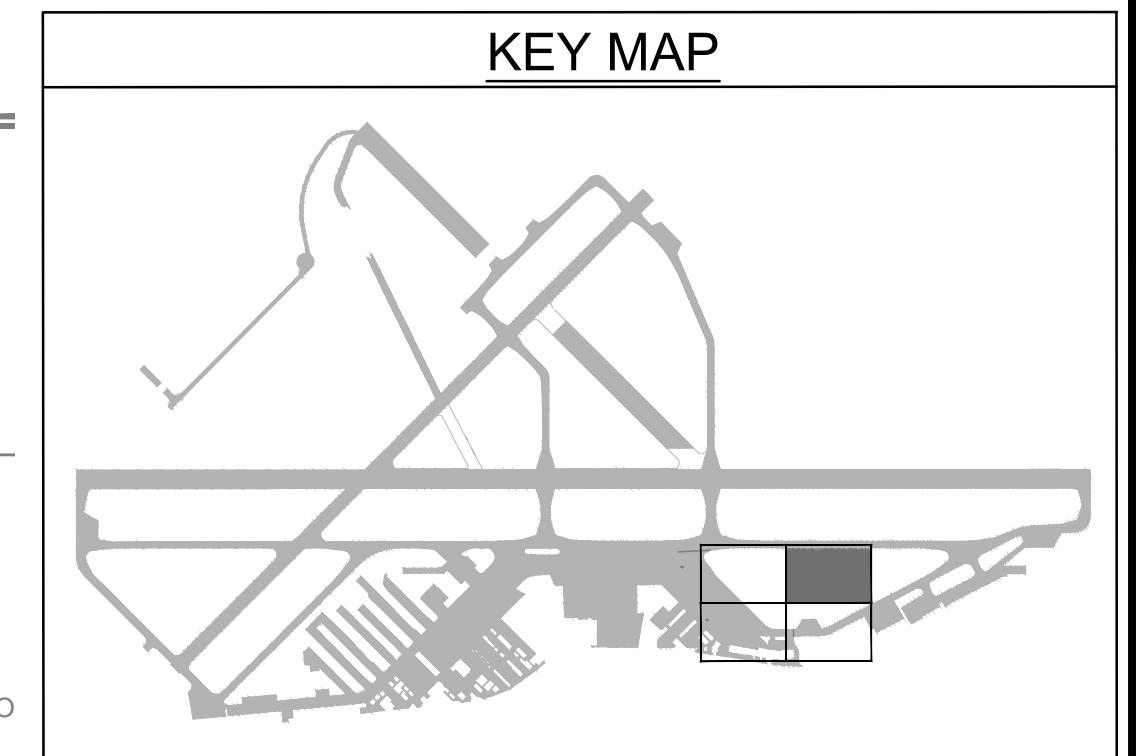


PCC JOINT NOTES

- ① DOWELED CONSTRUCTION JOINT PER DETAIL 5/C-341
- ② DOWELED CONTRACTION JOINT PER DETAIL 6/C-341
- ③ THICKENED EDGE JOINT PER DETAIL 1/C-341
- ④ PAVEMENT EDGE JOINT PER DETAIL 3/C-341
- ⑤ REINFORCED CONCRETE PANELS PER DETAIL 4/C-341

LEGEND:

- CONSTRUCTION JOINT
- DOWELED CONTRACTION JOINT
- THICKENED EDGE JOINT
- PAVEMENT EDGE
- REINFORCED CONCRETE
- VEHICLE TRACKING CONTROL AREA



PCC JOINT NOTES

- ① DOWELED CONSTRUCTION JOINT PER DETAIL 5/C-341
- ② DOWELED CONTRACTION JOINT PER DETAIL 6/C-341
- ③ THICKENED EDGE JOINT PER DETAIL 1/C-341
- ④ PAVEMENT EDGE JOINT PER DETAIL 3/C-341
- ⑤ REINFORCED CONCRETE PANELS PER DETAIL 4/C-341

LEGEND:

- CONSTRUCTION JOINT
- — — DOWELED CONTRACTION JOINT
- T — THICKENED EDGE JOINT
- PAVEMENT EDGE
-  REINFORCED CONCRETE
-  VEHICLE TRACKING CONTROL AREA

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—

KEY MAP



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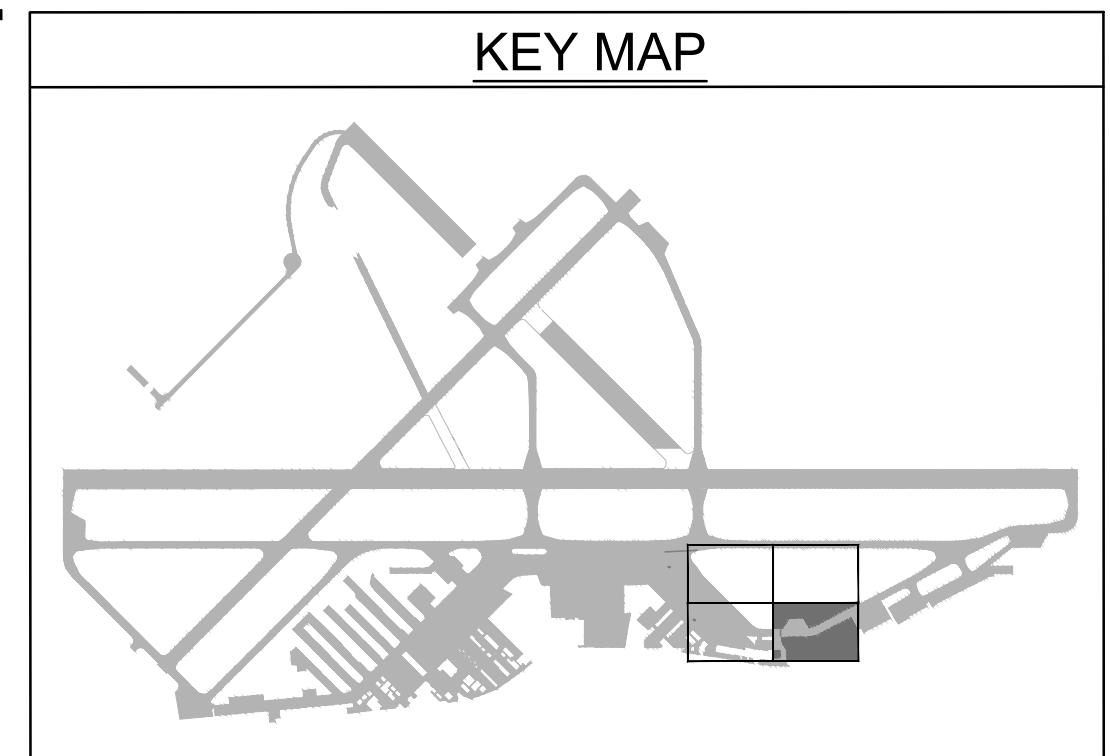
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JOINTING PLAN

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KEY MAP



PCC JOINT NOTE

- ① DOWELED CONSTRUCTION JOINT PER DETAIL 5/C-341
- ② DOWELED CONTRACTION JOINT PER DETAIL 6/C-341
- ③ THICKENED EDGE JOINT PER DETAIL 1/C-341
- ④ PAVEMENT EDGE JOINT PER DETAIL 3/C-341
- ⑤ REINFORCED CONCRETE PANELS PER DETAIL 4/C-341

LEGEND:

- — — — — CONSTRUCTION JOINT
- — — — — DOWELED CONTRACTION JOINT
- T — — — THICKENED EDGE JOINT
- — — — — PAVEMENT EDGE
-  REINFORCED CONCRETE
-  VEHICLE TRACKING CONTROL AREA

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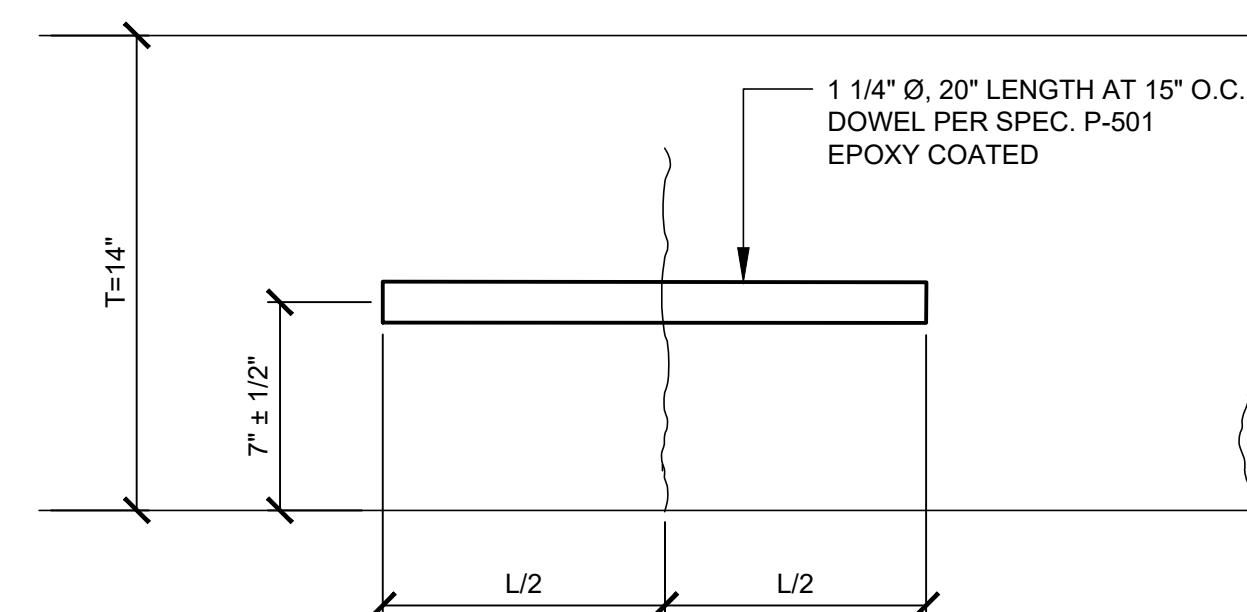
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JOINTING DETAILS

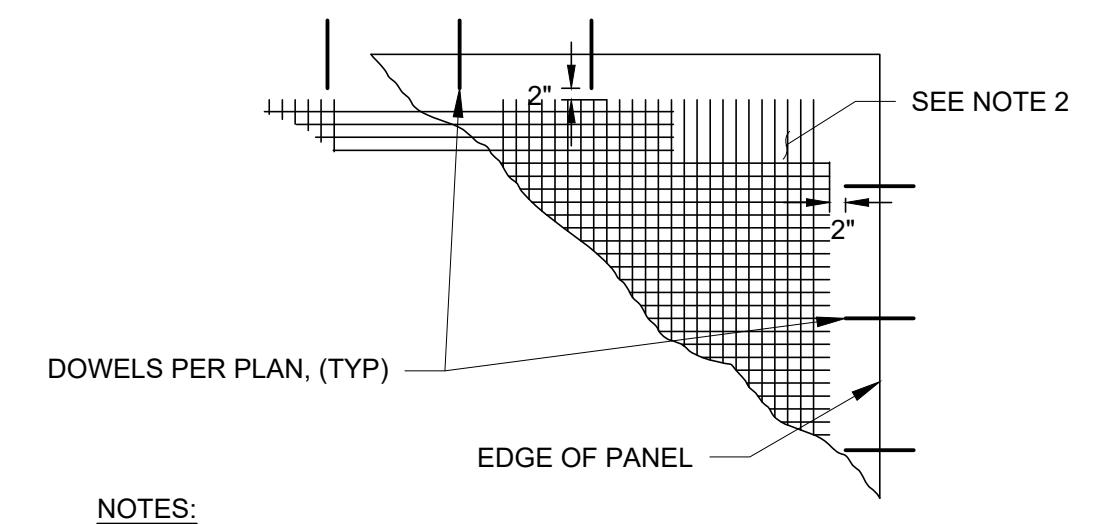
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C-341



7 SLAB THICKNESS AND BAR DIAMETER

NOT TO SCALE

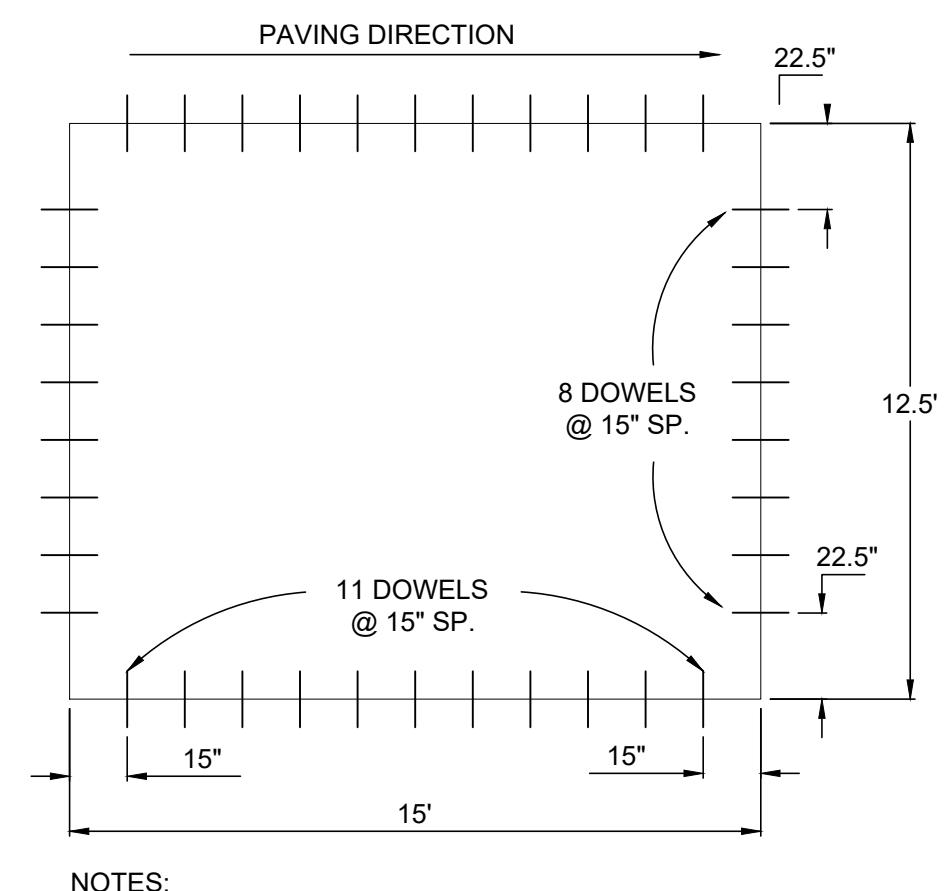


NOTES:

1. CONSTRUCT 6x6-W5xW5 or 12x12-W8.5xW8.5 WELDED WIRE FABRIC CONTINUOUS THROUGH PANEL.
2. WELDED WIRE FABRIC AT T/2 +1"
3. REINFORCING MESH TO BE OVERLAPPED MIN. 1 MESH LENGTH AND TIED.
4. BARS AND REINFORCING INCIDENTAL TO CONCRETE PAVEMENT.

4 REINFORCED CONCRETE PANELS

NOT TO SCALE

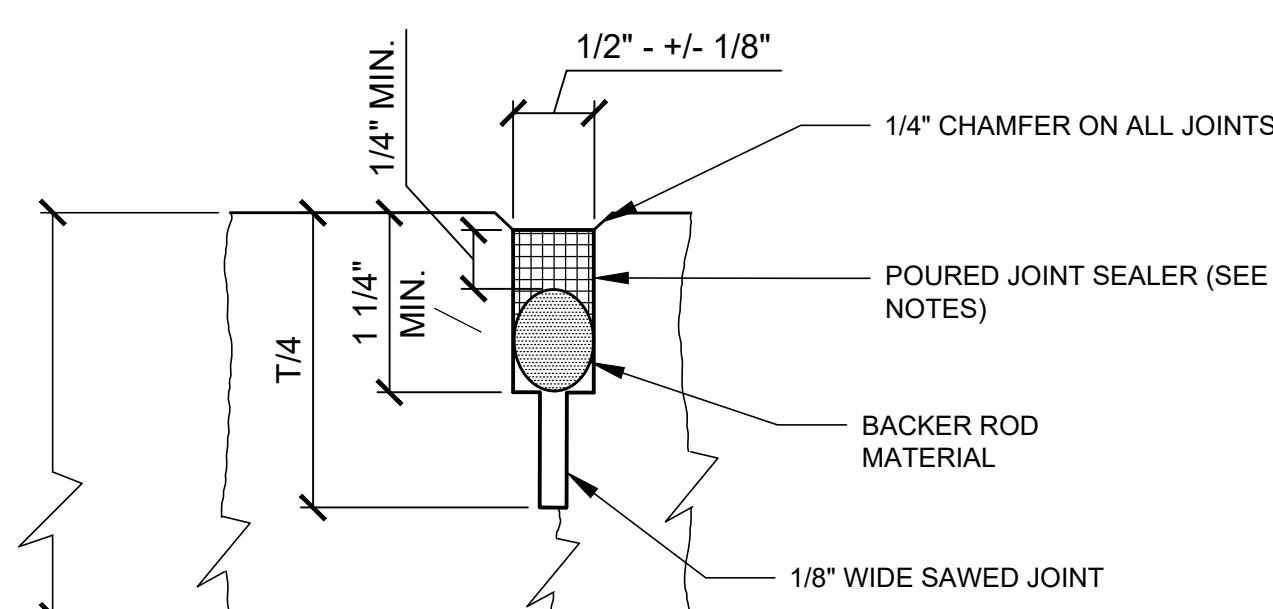


NOTES:

1. DETAIL SHOWS TYPICAL DOWEL LAYOUT ON ALL SIDES OF PANEL.
2. END OF DOWELS SHOULD NOT BE CLOSER THAN 10".
3. DO NOT DOWEL PAVEMENT FACES SHORTER THAN 30".

8 TYPICAL DOWEL SPACING FOR RECTANGULAR SLABS

NOT TO SCALE



NOTES:

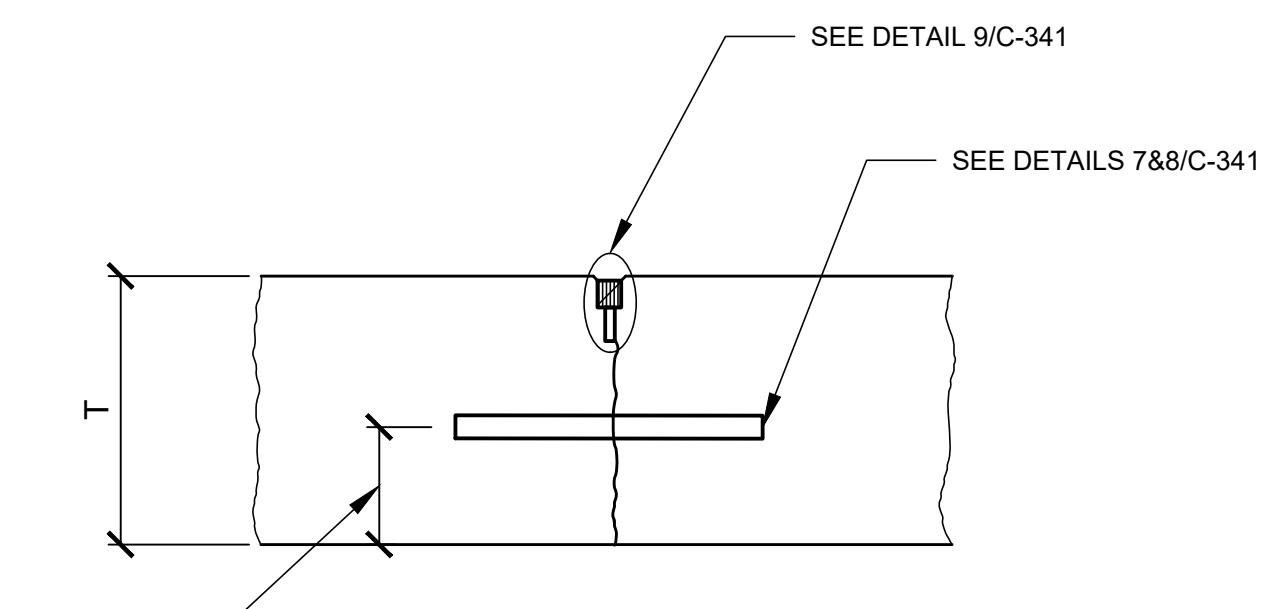
1. SEALANT RESERVOIR SIZED TO PROVIDE PROPER SHAPE FACTOR (WIDTH/DEPTH). DIFFERENT FIELD Poured SEALANTS REQUIRE DIFFERENT SHAPE FACTORS FOR OPTIMUM PERFORMANCE. USE MANUFACTURER'S RECOMMENDED SHAPE FACTOR.
2. ROD BACK-UP MATERIAL MUST BE COMPATIBLE WITH THE TYPE OF Poured SEALANT USED AND SIZED TO PROVIDE THE DESIRED SHAPE FACTOR.
3. EXISTING JOINTS SHALL BE THOROUGHLY CLEANED PER SPECIFICATIONS PRIOR TO APPLICATION OF JOINT SEAL.

9 CONSTRUCTION/CONTRACTION DETAIL

NOT TO SCALE

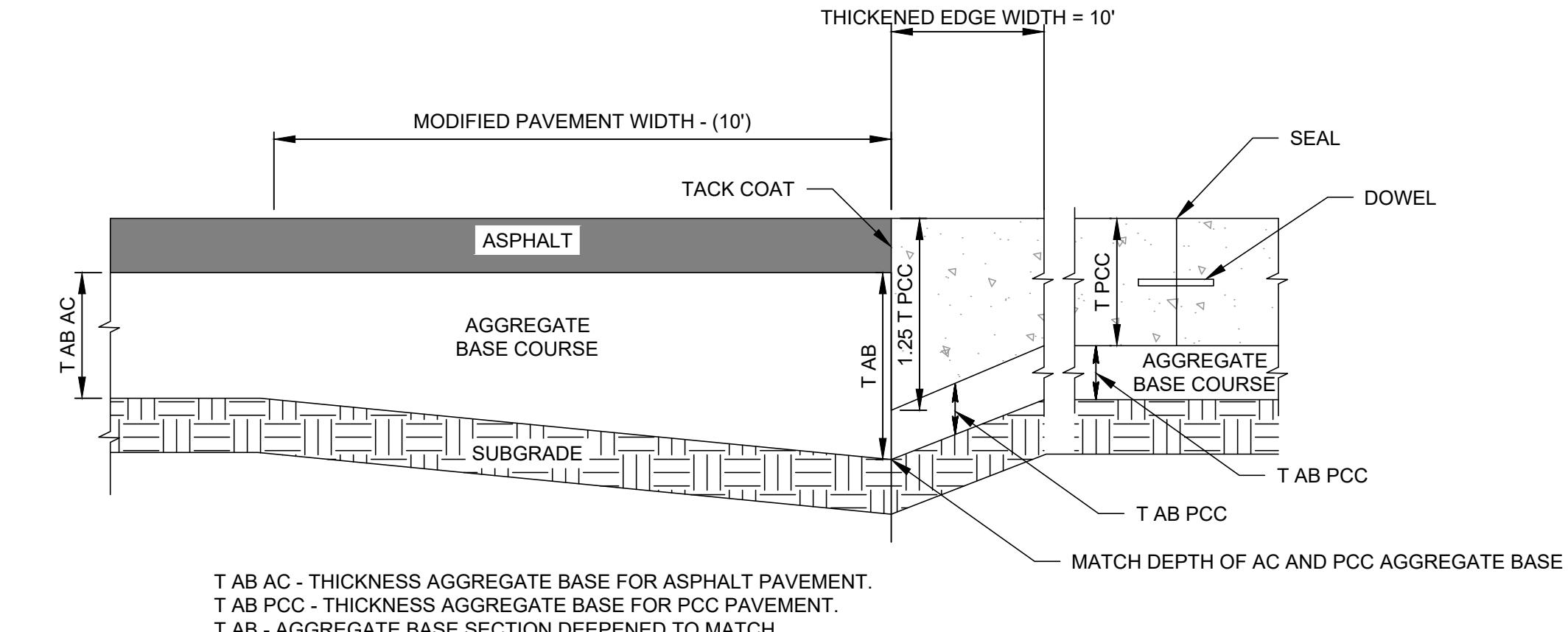
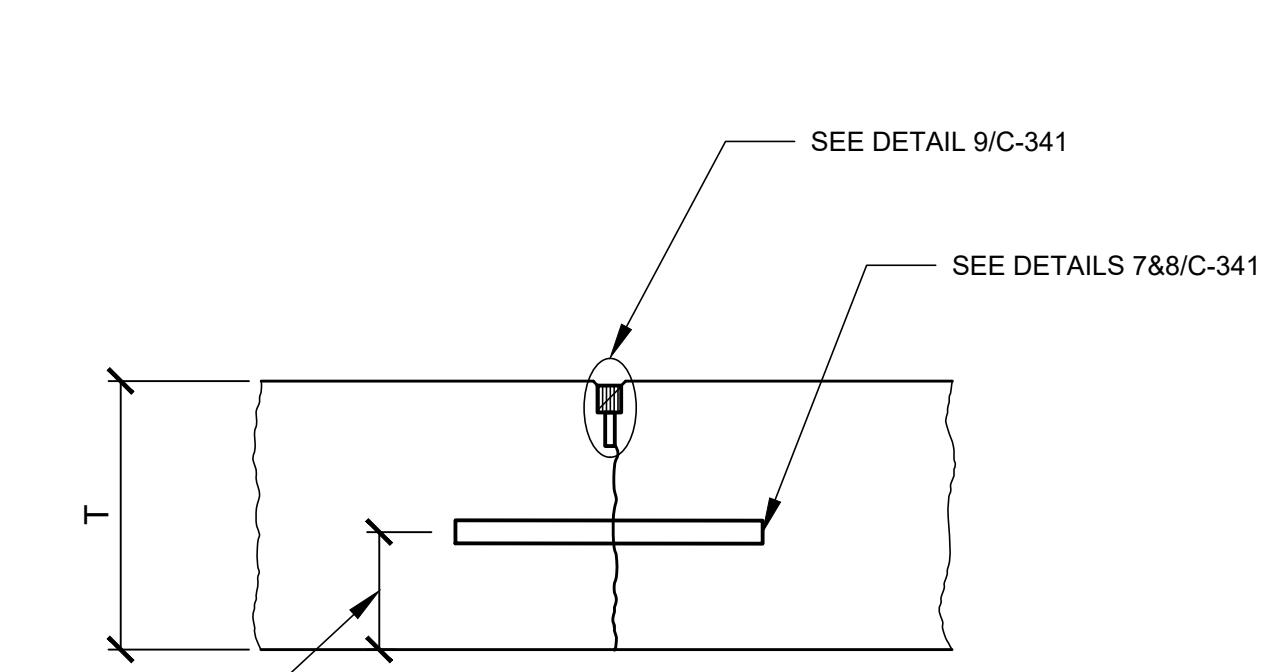
5 DOWELED CONSTRUCTION JOINT

NOT TO SCALE



6 DOWELED CONTRACTION JOINT

NOT TO SCALE



NOTE:

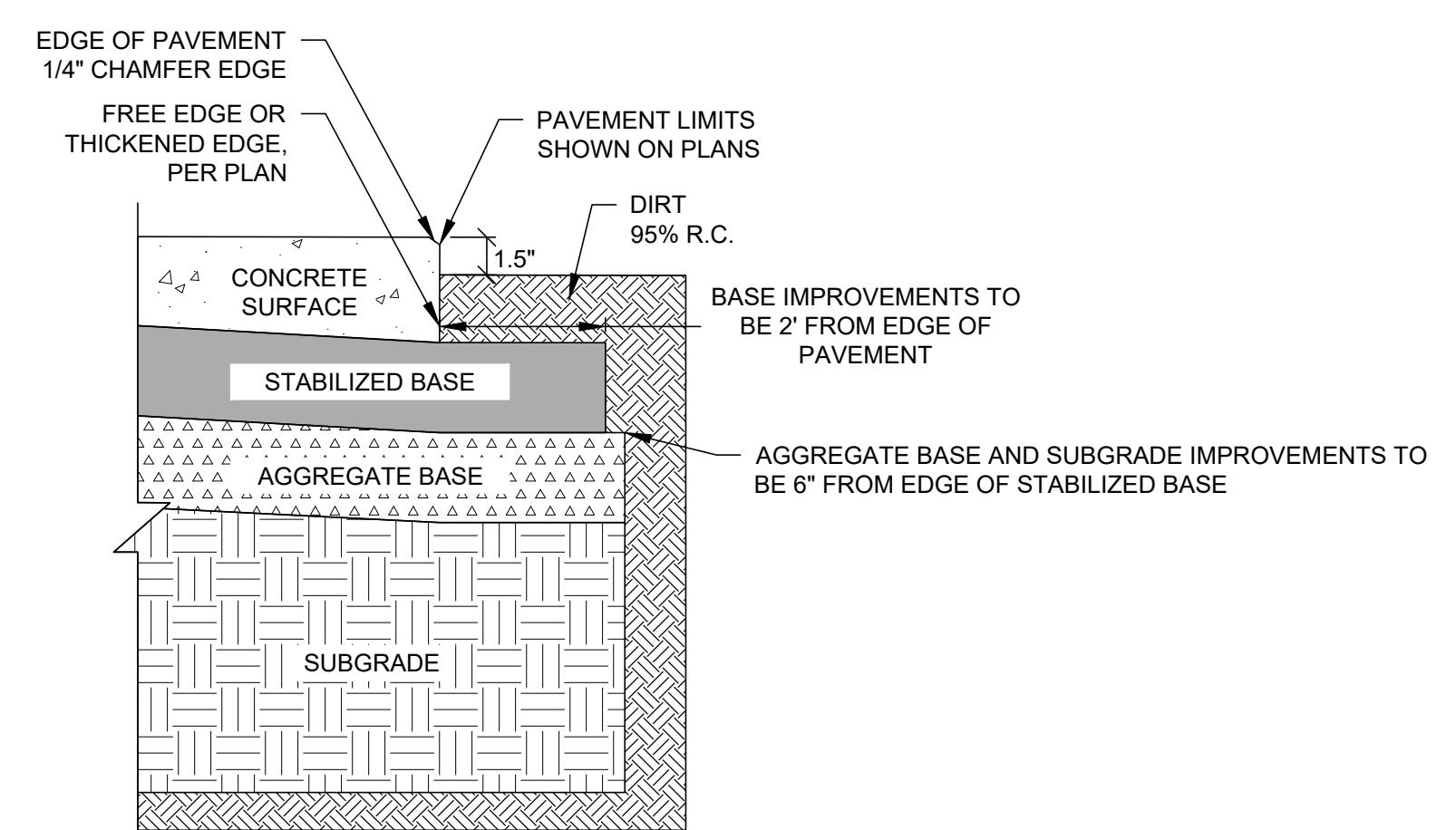
1. ACC/PCC SECTION SHOWN. FOR FREE PCC EDGE, CONSTRUCT THICKENED EDGE ATOP OVERBUILT STABILIZED BASE AND AGGREGATE BASE PER DETAIL 3/C-341.

1 THICKENED EDGE JOINT

NOT TO SCALE

2 NOT USED

NOT TO SCALE



NOTE:

1. CONSTRUCT A 1.5 INCH DROP BETWEEN PAVED AND UNPAVED SURFACES.

3 PAVEMENT EDGE CONDITION

NOT TO SCALE

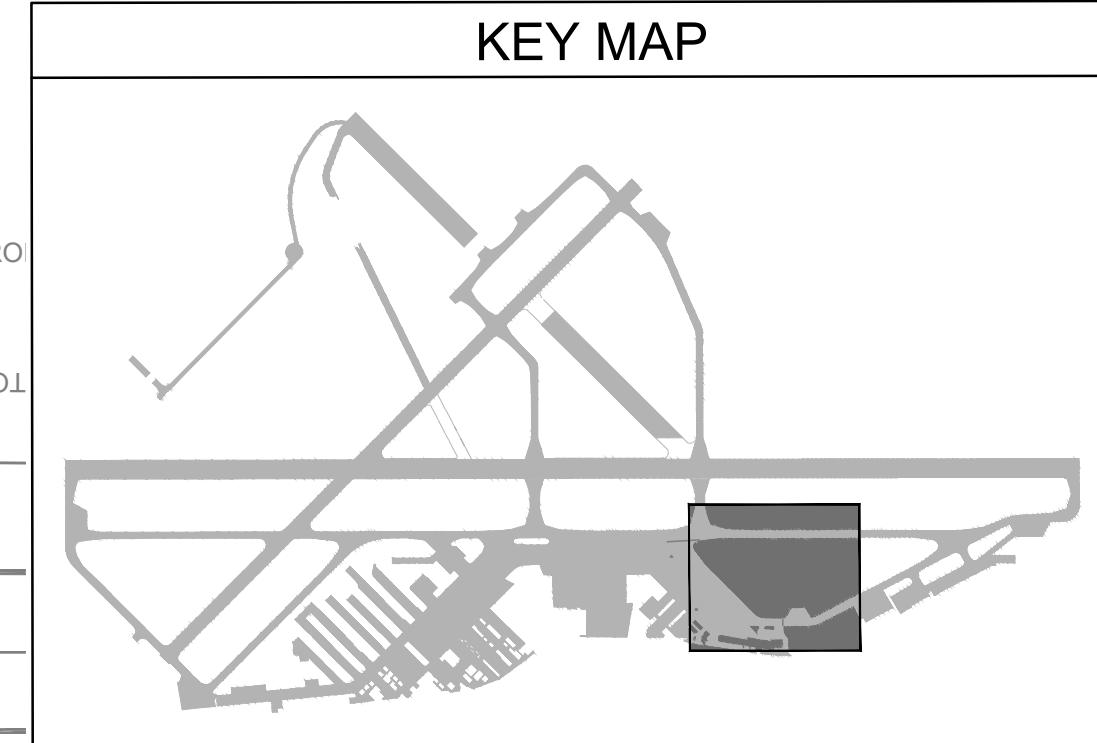


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KEY MAP



PAVEMENT MARKING NOTES:

- ① PAINT DASHED TAXIWAY EDGE MARKING PER DETAIL 1/C-671
- ② PAINT INTERMEDIATE HOLDING POSITION MARKING PER DETAIL 2/C-671
- ③ PAINT CONTINUOUS TAXIWAY EDGE MARKING PER DETAIL 1/C-671
- ④ PAINT TAXIWAY CENTERLINE MARKING PER DETAIL 4/C-671
- ⑤ CONSTRUCT L-853 RETRO-REFLECTIVE TAXIWAY EDGE MARKER PER DETAIL 3/C-671

GENERAL PAVEMENT MARKING NOTES:

1. ALL NEW YELLOW MARKINGS SHALL RECEIVE BLACK OUTLINE.
2. ALL MARKINGS, EXCLUDING BLACK PAINT, SHALL RECEIVE GLASS BEADS UNLESS OTHERWISE INDICATED ON THE PLANS. BLACK OUTLINE SHALL NOT HAVE BEADS.
3. ALL LAYOUT PAINT TRACKS SHALL BE LOCATED SO THAT THE NEW MARKING WILL FULLY COVER LAYOUT LINES.
4. ALL MARKINGS, ON EXISTING PAVEMENTS WHERE WORK HAS DISTURBED BLACK MARKINGS ARE TO RECEIVE ONE COAT WITHOUT BEADS.

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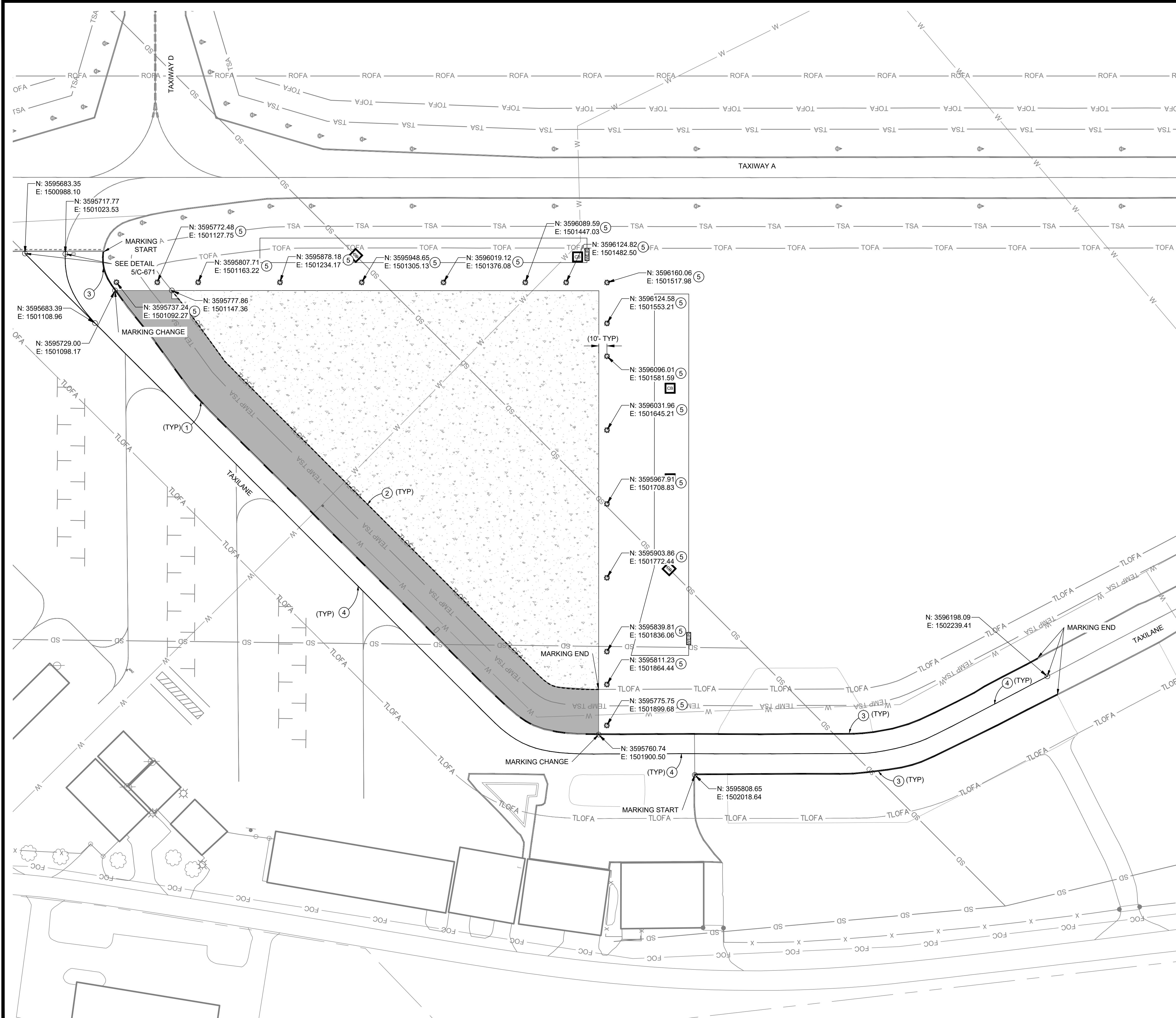
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SHEET CONTENTS
MARKING & SIGNAGE
PLAN

SHEET NO.
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C-651



NOTE:
TWO TYPICAL MODELS SHOWN. CONTRACTOR MAY SUBMIT ANY FAA-APPROVED L-853 TAXIWAY RETROREFLECTIVE MARKER

L-853 TAXIWAY EDGE RETROREFLECTOR DETAIL

The diagram illustrates a continuous yellow centerline on a taxiway. The centerline is 6 inches thick and is surrounded by a 6-inch black outline. The total width of the yellow centerline and its black outline is 18 inches. The taxiway is 100 feet long. On either side of the yellow centerline is a 6-inch thick shoulder, and on either side of each shoulder is a 6-inch thick shoulder shoulder. The total width of the taxiway, including the shoulders and shoulder shoulders, is 132 inches (11 feet). The diagram is labeled with 'CONTINUOUS' and 'YELLOW PAINT'.

1 TAXIWAY EDGE MARKING
NOT TO SCALE

The diagram shows a horizontal yellow line with a dashed center. This line is surrounded by two thick black lines, each 6 inches wide. The total width of the yellow line and its black outlines is 12 inches. Arrows point to the text labels: 'N/E OR STA/OFF FROM CENTER OF MARKING PER PLANS' at the top left, '6" BLACK OUTLINE' on the top right, '6" BLACK OUTLINE' at the bottom right, and '6" YELLOW PAINT' at the top right. A small 'C' is located on the left side of the yellow line.

4 TAXIWAY CENTERLINE MARKING

NOT TO SCALE

N/E OR STA/OFF FROM
CENTER OF MARKING PER PLANS

DASHES AND SPACES
3'X1'

The diagram illustrates the transition from an 'EXISTING' marking to a 'PROPOSED' one. On the left, a 'EXISTING' marking is shown as a thick yellow line with a 6" black outline, positioned 6" above the ground. On the right, a 'PROPOSED' marking is shown as a thick yellow line with a 6" black outline, positioned 6" above the ground. The 'PROPOSED' marking includes a 'HOLD SIDE' section and a 'BLACK OUTLINE' section. A callout points to the 'TAXIWAY CENTERLINE MARKING' with the note 'SEE DETAIL 4/C-671'. Another callout points to the 'TAXIWAY CENTERLINE YELLOW MARKING' with the note 'DIRECTLY ABUTS BLACK BACKGROUND OF NON-MOVEMENT AREA BOUNDARY'. Arrows indicate the 6" height and the 6" width of the markings.

NON-MOVEMENT AREA BOUNDARY MARKING

2 INTERMEDIATE HOLDING POSITION MARKING

NOT TO SCALE

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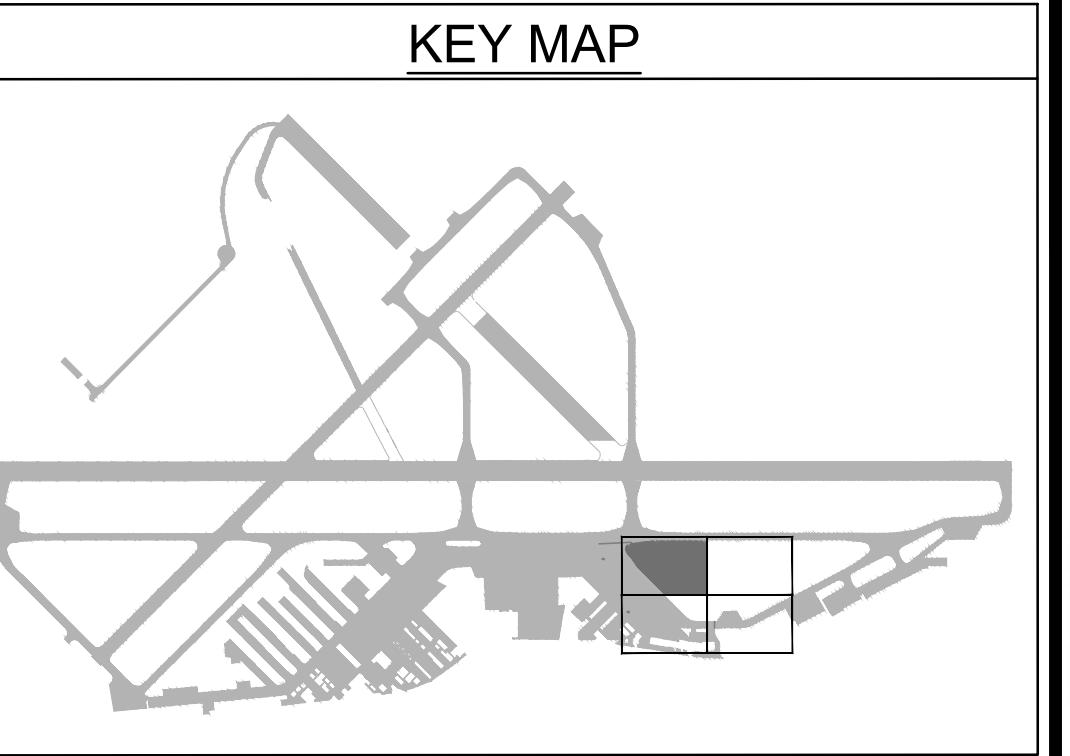
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JOINT ELEVATION
PLAN

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KEY MAP

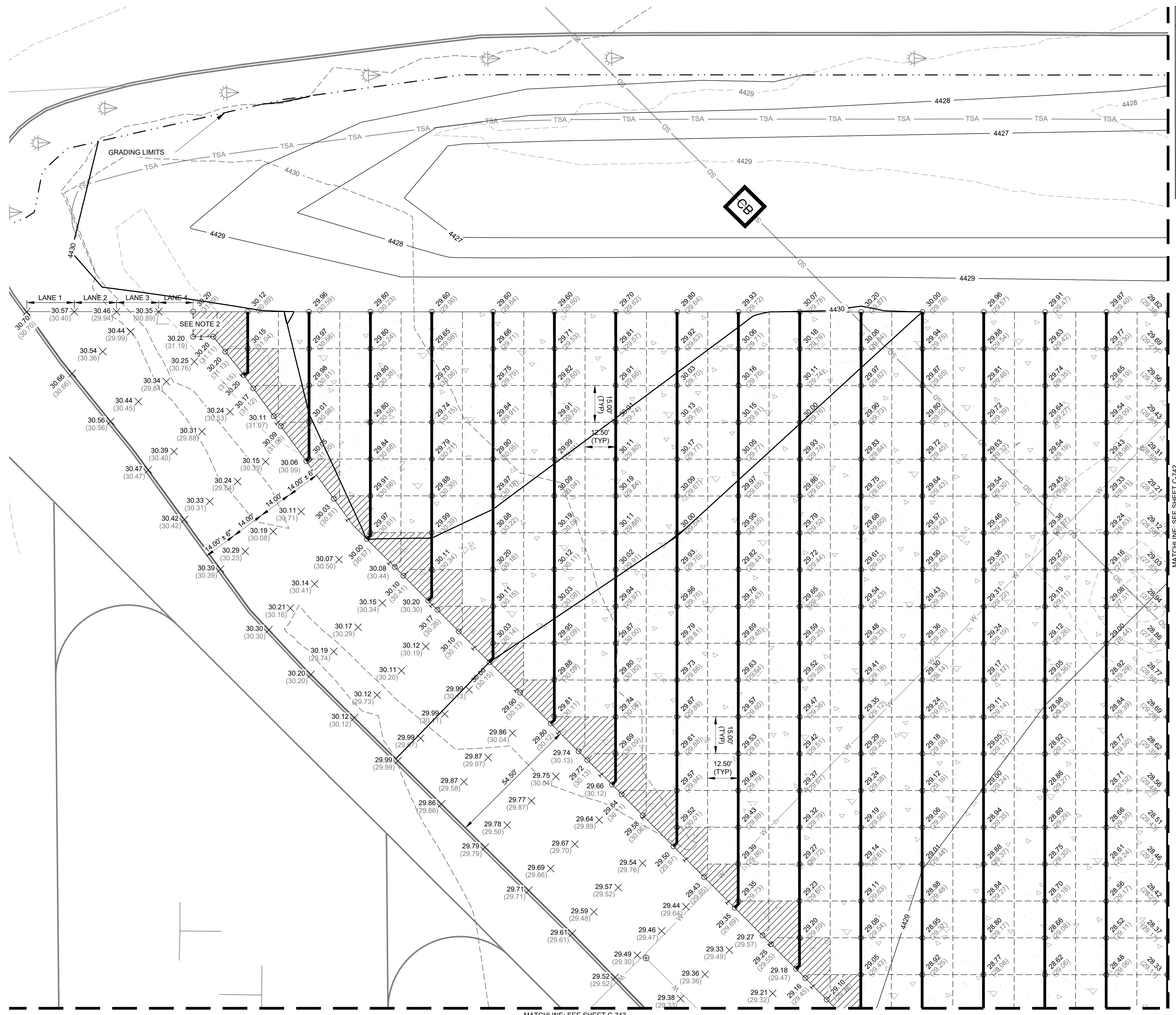


NOTES:

1. PCC GRADES SHOWN ARE ALONG PCC PAVING LANE CONSTRUCTION JOINTS.
2. AC GRADE SHOWN ARE ALONG AC PAVING GRID ESTABLISHED AT 25' INTERVALS ON LANE 3/4. ALL OTHER AC LANES ARE PERPENDICULAR OFFSET.

LEGEND:	
	CONSTRUCTION JOINT
	DOWELED CONTRACTION JOINT
	THICKENED EDGE JOINT
	PAVEMENT EDGE
	GRADING LIMIT
	REINFORCED CONCRETE
	VEHICLE TRACKING CONTROL AREA
	PCC GRADE
	AC GRADE

Matchline: SEE SHEET C-742





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SHEET CONTENTS
JOINT ELEVATION
Elevation

SHEET NO. 1

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C-743

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KEY MAP

MATCHLINE: SEE SHEET C-741

GRADING LIMITS

101

MAG

TLOFA

SD

GS

M

X

Y

Z

MATCHLINE: SEE SHEET C-744



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