

# OGDEN-HINCKLEY AIRPORT

## RUNWAY 3-21 AND TAXIWAY PAVEMENT MAINTENANCE

3909 AIRPORT RD  
OGDEN, UT 84405  
AIP NO.: 3-49-0024-067-2025  
ISSUED FOR BID  
APRIL 1, 2025

**Mead & Hunt**  
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DATE: 04/01/2025

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M&H NO.: 3132200-250388.01  
DATE: APRIL 1, 2025  
DESIGNED BY: CJS  
DRAWN BY: JAL  
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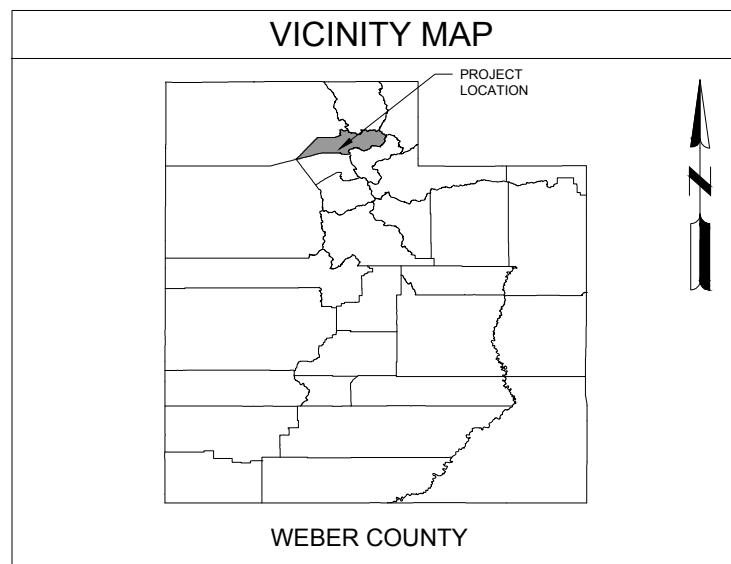
COVER SHEET

SHEET NO. 1 of 22

G-001



Know what's below.  
Call before you dig.





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## QUANTITIES

SCHEDULE 1 - RUNWAY AND TAXIWAY PAVEMENT MAINTENANCE			
ITEM	ITEM DESCRIPTION	UNITS	QUANTITY
C-105-6.1	Mobilization (10% Maximum)	LS	1
P-101-5.1	Surface Preparation, Waterblast Existing Markings	LS	1
P-605-5.1	Joint and Crack Repair	LF	60,000
P-620-5.1a	Pavement Marking, Yellow with Reflective Media	SF	9,200
P-620-5.1b	Pavement Marking, Yellow without Reflective Media	SF	6,600
P-620-5.1c	Pavement Marking, White, with Reflective Media	SF	126,000
P-620-5.1e	Pavement Marking, Black without Reflective Media	SF	49,100
P-620-5.2	Reflectance Testing	LS	1
P-620-5.3	Marking Removal, by Waterblast	SF	4,390
L-125-5.1	Remove and Salvage Guidance Sign	LS	1
L-125-5.2	Remove and Relocate Guidance Sign	LS	1
P-608-R-8.1a	Asphalt Surface Treatment with Aggregate	SY	146,400
P-608-R-8.1b	Asphalt Surface Treatment without Aggregate	SY	26,900
P-608-R-8.1c	Asphalt Surface Treatment without Aggregate at Paving Joint	SY	7,500
P-608-R-8.2	Runway Friction Testing	LS	1
SCHEDULE 2 - PARKING LOT PAVEMENT MAINTENANCE (NON-FEDERAL)			
ITEM	ITEM DESCRIPTION	UNITS	QUANTITY
P-608-R-8.1b	Asphalt Surface Treatment without Aggregate	SY	9,300
P-620-5.1d	Permanent Pavement Marking, White without Reflective Media	SF	1,500

## GENERAL NOTES

- IF DURING THE CONSTRUCTION PROCESS, CONDITIONS ARE ENCOUNTERED WHICH COULD INDICATE A SITUATION THAT IS NOT IDENTIFIED IN THE PLANS OR SPECIFICATIONS, THE CONTRACTOR SHALL CONTACT THE RPR IMMEDIATELY.
- ALL REFERENCES TO ANY PUBLISHED STANDARDS SHALL REFER TO THE LATEST REVISION OF SAID STANDARDS, UNLESS SPECIFICALLY STATED OTHERWISE.
- WHENEVER, IN THE CONTRACT DOCUMENTS, THE WORDS "PROVIDE", "FURNISH", "INSTALL", "FURNISH AND INSTALL", OR SIMILAR WORDS ARE USED, IT SHALL BE UNDERSTOOD THAT THE INTENT OF THE CONTRACT DOCUMENTS IS TO PROVIDE FOR THE CONSTRUCTION AND COMPLETION IN EVERY DETAIL THE WORK DESCRIBED. IT IS FURTHER INTENDED THAT THE CONTRACTOR SHALL FURNISH ALL LABOR, SUPERVISION, MATERIALS, EQUIPMENT, TOOLS, TRANSPORTATION, SUPPLIES, TESTING, AND INCIDENTALS REQUIRED TO COMPLETE THE WORK IN ACCORDANCE WITH THE DRAWINGS (PLANS), SPECIFICATIONS, AND TERMS OF THE CONTRACT.
- CONTRACTOR SHALL KEEP A SET OF AS-BUILT DRAWINGS ON-SITE AND MAKE AVAILABLE TO THE RPR AT ALL TIMES. AS-BUILT SET SHALL BE SUBMITTED TO THE RPR AT THE COMPLETION OF THE JOB. CONTRACTOR SHALL BE RESPONSIBLE FOR RECORDING ALL AS-BUILT INFORMATION DURING THE PROJECT. THE CONTRACTOR SHALL NOTE, AND BRING TO THE RPR'S ATTENTION, ANY DISCREPANCIES BETWEEN THE CONTRACT DOCUMENTS AND ACTUAL FIELD CONDITIONS.
- ALL DAMAGE TO UTILITIES, PAVEMENT, EQUIPMENT, OR STRUCTURES FROM CONSTRUCTION ACTIVITIES SHALL BE IMMEDIATELY REPORTED TO THE RPR. THE RPR SHALL DETERMINE WHETHER REPAIR OR REPLACEMENT IS NECESSARY. ALL REPAIR METHODS SHALL BE SUBMITTED TO THE RPR FOR REVIEW AND APPROVAL PRIOR TO INITIATING THE WORK. REPAIRS SHALL BE MADE TO THE APPROVAL OF THE RPR AND AT NO ADDITIONAL COST TO THE SPONSOR.
- THE CONTRACTOR SHALL PROVIDE WORKMANSHIP AND MATERIALS THAT ARE OF GOOD QUALITY AND COMPLY WITH THE REQUIREMENTS OF THE CONTRACT DOCUMENTS.
- THE CONTRACTOR SHALL PROVIDE WORK, EQUIPMENT AND MATERIALS THAT COMPLY WITH FAA REQUIREMENTS, NATIONAL ELECTRICAL CODE, NATIONAL ELECTRICAL SAFETY CODE, AND ALL LOCAL CODES.
- CONTRACTOR SHALL PROVIDE THE NECESSARY NUMBER OF RADIOS FOR HIS/HER WORKFORCE. RADIOS SHALL BE INSPECTED DAILY AND REMAIN CHARGED AND IN GOOD WORKING CONDITION AT ALL TIMES.
- TEN (10) DAYS PRIOR TO THE BEGINNING OF WORK, THE CONTRACTOR SHALL SUBMIT A WORK SCHEDULE CONSISTENT WITH THE PHASING PLAN STATED IN THE DESIGN.
- THE CONTRACTOR SHALL HAVE A MINIMUM OF ONE (1) CURRENT COPY OF THE APPROVED PLANS (INCLUDING ANY CHANGE ORDERS, SUPPLEMENTAL AGREEMENTS, FIELD DIRECTIVES, ETC.), ONE (1) CURRENT COPY OF THE APPROPRIATE STANDARDS AND SPECIFICATIONS, AND A COPY OF ANY PERMITS AND EXTENSION AGREEMENTS NEEDED FOR THE JOB, ON SITE AT ALL TIMES.
- DIMENSIONING FOR LAYOUTS AND CONSTRUCTION ARE NOT TO BE SCALED FROM ANY DRAWINGS. IF PERTINENT DIMENSIONS ARE NOT SHOWN, CONTACT THE RPR FOR CLARIFICATION.

## UTILITIES

- THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES, EXISTING DUCT BANKS, CIRCUITING, AND STRUCTURES AS SHOWN ON THESE PLANS, IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES AND, WHERE POSSIBLE, BASED ON AVAILABLE INFORMATION OR MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED UPON AS BEING EXACT OR COMPLETE, NOR IS IT WARRANTED THAT ALL ITEMS ARE SHOWN. THE EXISTING UTILITY LOCATIONS SHOWN ON THE PLANS SHALL NOT BE SCALLED FOR EXACT LOCATIONS.

## SUBMITTALS

- THE CONTRACTOR SHALL SUBMIT A DETAILED LISTING OF ALL SUBMITTALS (E.G., MIX DESIGNS, MATERIAL CERTIFICATION, AND PRODUCT INFORMATION) AND SHOP DRAWINGS REQUIRED BY THE TECHNICAL SPECIFICATIONS.
- THE CONTRACTOR SHALL REVIEW THE CONTRACT DOCUMENTS SECTION 100-05 FOR SUBMITTAL SCHEDULE REQUIREMENTS.
- THE CONTRACTOR SHALL PROVIDE MATERIAL SUBMITTALS FOR THE RPR'S APPROVAL AT LEAST TEN (10) DAYS PRIOR TO ORDERING.

## SITE ACCESS

- DURING CONSTRUCTION, THE CONTRACTOR SHALL MINIMIZE DISTURBANCES TO ALL CONSTRUCTION AREAS AND ACCESS ROUTES. THIS INCLUDES EQUIPMENT AND VEHICULAR RUTS CREATED IN ANY PAVEMENTS, ANY HAUL/ACCESS ROADS, OR ANY INFIL/SAFETY AREAS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO EXISTING FACILITIES OR ROADS. REPAIRS SHALL BE MADE AT NO ADDITIONAL COST TO THE SPONSOR AND TO THE SATISFACTION OF THE RPR.
- CONTRACTOR SHALL EXAMINE THE EXISTING PAVEMENTS THAT WILL BE USED FOR HAULING OF MATERIAL AND EQUIPMENT AND DETERMINE THE PAVEMENT'S ABILITY TO WITHSTAND CONTRACTOR OPERATIONS WITHOUT CAUSING DAMAGE TO THE PAVEMENT. ANY DAMAGE CAUSED BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR TO THE APPROVAL OF THE RPR AND AT NO ADDITIONAL COST TO THE SPONSOR.

- AT NO TIME SHALL LIGHT PLANTS BE LEFT RUNNING WHEN CONSTRUCTION OPERATIONS ARE NOT IN PROCESS.
- ALL CONTRACTOR EMPLOYEES SHALL BE REQUIRED TO PARK IN THE CONTRACTOR'S DESIGNATED STAGING AREA ONLY AND SHALL BE DRIVEN TO THE PROJECT SITE BY DESIGNATED CONSTRUCTION VEHICLES. ALL DRIVERS SHALL COMPLETE THE REQUIRED AIRFIELD DRIVING TRAINING BEFORE BEING ALLOWED TO DRIVE ON THE AIRPORT.

- THE CONTRACTOR SHALL BE AWARE THAT OTHER CONSTRUCTION MAY BE ACTIVE DURING THIS PROJECT. COORDINATION WILL BE REQUIRED WITH AIRPORT STAFF AND VARIOUS CONTRACTORS THROUGH THE RPR.
- DURING CONSTRUCTION, THE CONTRACTOR SHALL COMPLY WITH FAA ADVISORY CIRCULAR (AC) 150/5370-2G, "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION".

- THE CONTRACTOR SHALL REVIEW THE CONSTRUCTION SAFETY AND PHASING PLAN (CSP) CONTAINED IN THE CONTRACT DOCUMENTS. ADVISORY CIRCULAR (AC) 150/5370-2G, "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION" REQUESTS THE CONTRACTOR TO PREPARE A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) PRIOR TO NTP FOR APPROVAL BY THE RPR.
- ALL VEHICLES AND EQUIPMENT WORKING REGULARLY ON THE PROJECT SITE SHALL BE REQUIRED TO BE EQUIPPED WITH STANDARD FAA MARKINGS PER FAA ADVISORY CIRCULAR 150/5210-5 OR BE ESCORTED BY A PROPERLY MARKED VEHICLE. A FLASHING BEACON MAY BE USED AT ANY TIME. FAILURE TO PROVIDE SUCH MARKINGS OR ESCORT FOR ANY EQUIPMENT INSIDE THE AIRPORT PERIMETER FENCE WILL PRECLUDE THAT EQUIPMENT FROM

OPERATING ON THE PROJECT. DELAYS CAUSED DUE TO LACK OF CONFORMANCE SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. BACKUP ALARMS SHALL BE ADJUSTED FOR SURROUNDING NOISE LEVELS.

- THE CONTRACTOR SHALL PROVIDE FOUR (4) EXISTING PORTABLE LIGHTED RUNWAY CLOSURE CROSSES (RCC'S). THE CONTRACTOR IS RESPONSIBLE FOR PLACEMENT AND REMOVAL OF THE RCC'S DURING CONSTRUCTION, AS WELL AS TRANSPORTATION OF THE RCC'S TO AND FROM THE STORAGE AREA WHEN NOT IN USE.
- THE CONTRACTOR WILL BE RESPONSIBLE FOR ALL MAINTENANCE OF THE RCC'S DURING THE WORK. THIS SHALL INCLUDE, BUT NOT BE LIMITED TO, FUEL, OIL, GREASE, BULB REPLACEMENT, AND/OR REPAIR.
- UPON COMPLETION OF THE WORK, THE CONTRACTOR SHALL SERVICE THE RCC'S BY REFUELING, CHANGING OIL/BULB REPLACEMENT, AND OTHER REQUIREMENTS IDENTIFIED IN THE SERVICE MANUAL. RCC'S SHALL BE RETURNED TO THE AIRPORT IN GOOD WORKING CONDITION AND THE CONTRACTOR WILL BE RESPONSIBLE FOR REPLACEMENT OF DAMAGED PARTS/UNITS AS A RESULT OF CONTRACTOR'S USE.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL ASPECTS OF SAFETY INCLUDING, BUT NOT LIMITED TO, EXCAVATION, TRENCHING, SHORING, TRAFFIC CONTROL, AND SECURITY.

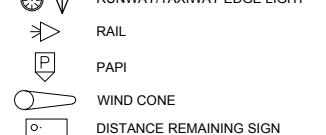
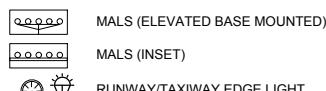
## PERMITTING

- THE CONTRACTOR SHALL SUBMIT A COPY OF ALL PERMITS REQUIRED FOR THE PROJECT TO THE RPR, FOR HIS REVIEW.

## LINES

— BRL —	BOUNDARY (PROJECT/CONSTRUCTION LIMITS)	ALT	ALTERNATE
— OHP —	BUILDING RESTRICTION LINE	AOA	AIR OPERATIONS AREA
— E —	ELECTRIC, EXISTING OVERHEAD	APPROX	APPROXIMATE
— FAA —	ELECTRIC, UNDERGROUND	ARFF	AIRCRAFT RESCUE AND FIRE FIGHTING
— X —	FAA OWNED UTILITY	ACT	AIR TRAFFIC CONTROL TOWER
— GSCA —	FENCE	ATO	AIR TRAFFIC ORGANIZATION
— LCA —	GLIDE SLOPE CRITICAL AREA	BLDG	BUILDING
— OFA —	LOCALIZER CRITICAL AREA	BRL	BUILDING RESTRICTION LINE
— — —	OBJECT FREE AREA	CP	CONTROL POINT
— ROFA —	PROPERTY LINE	CSPP	CONSTRUCTION SAFETY AND PHASING PLAN
— RPZ —	RUNWAY OBJECT FREE AREA	DEG	DEGREE
— RSA —	RUNWAY PROTECTION ZONE	DIA	DIAMETER
— SS —	RUNWAY SAFETY AREA	DIM	DIMENSION
— SIDA —	(E)	EXISTING	
— S —	SECURITY IDENTIFICATION DISPLAY AREA	E	EASTING
— SD —	SIGNAL CABLE, UNDERGROUND	ELEV	ELEVATION
— TOFA —	STORM DRAIN / CULVERT	FAA	FEDERAL AVIATION ADMINISTRATION
— TSA —	TAXIWAY OBJECT FREE AREA	FT	FEET
— OHT —	TAXIWAY SAFETY AREA	GSCA	GLIDE SLOPE CRITICAL AREA
— T —	TELEPHONE, OVERHEAD	IN	INCHES
— UD —	UNDERDRAIN	LCA	LOCALIZER CRITICAL AREA
— TD —	TRENCH DRAIN	LF	LINEAR FEET
— W —	WATER	LT	LIGHT

## EXISTING ELECTRIC



## SYMBOLS

◆	BENCHMARK
E	ELECTRIC MANHOLE
E	ELECTRIC HANHOLE (PULLBOX)
G	GAS MANHOLE
G	GATE
HH	HANDHOLE, GENERIC
S	SANITARY SEWER MANHOLE
CB	CATCH BASIN, ROUND
CB	CATCH BASIN, SQUARE
D	STORM DRAIN MANHOLE
T	TELECOMMUNICATIONS MANHOLE
T	TELECOMMUNICATIONS PEDESTAL BOX
UD	UNDERDRAIN MANHOLE
W	WATER MANHOLE
—	SECTION REFERENCE

(SEE XX-XXX) DETAIL REFERENCE

## PATTERNS

■	EMULSIFIED ASPHALT SEAL COAT WITH AGGREGATE, P-608-R (RUNWAY & TAXIWAY)
■	EMULSIFIED ASPHALT SEAL COAT WITHOUT AGGREGATE, P-608-R (SHOULDERS, BLAST PADS)

## GENERAL



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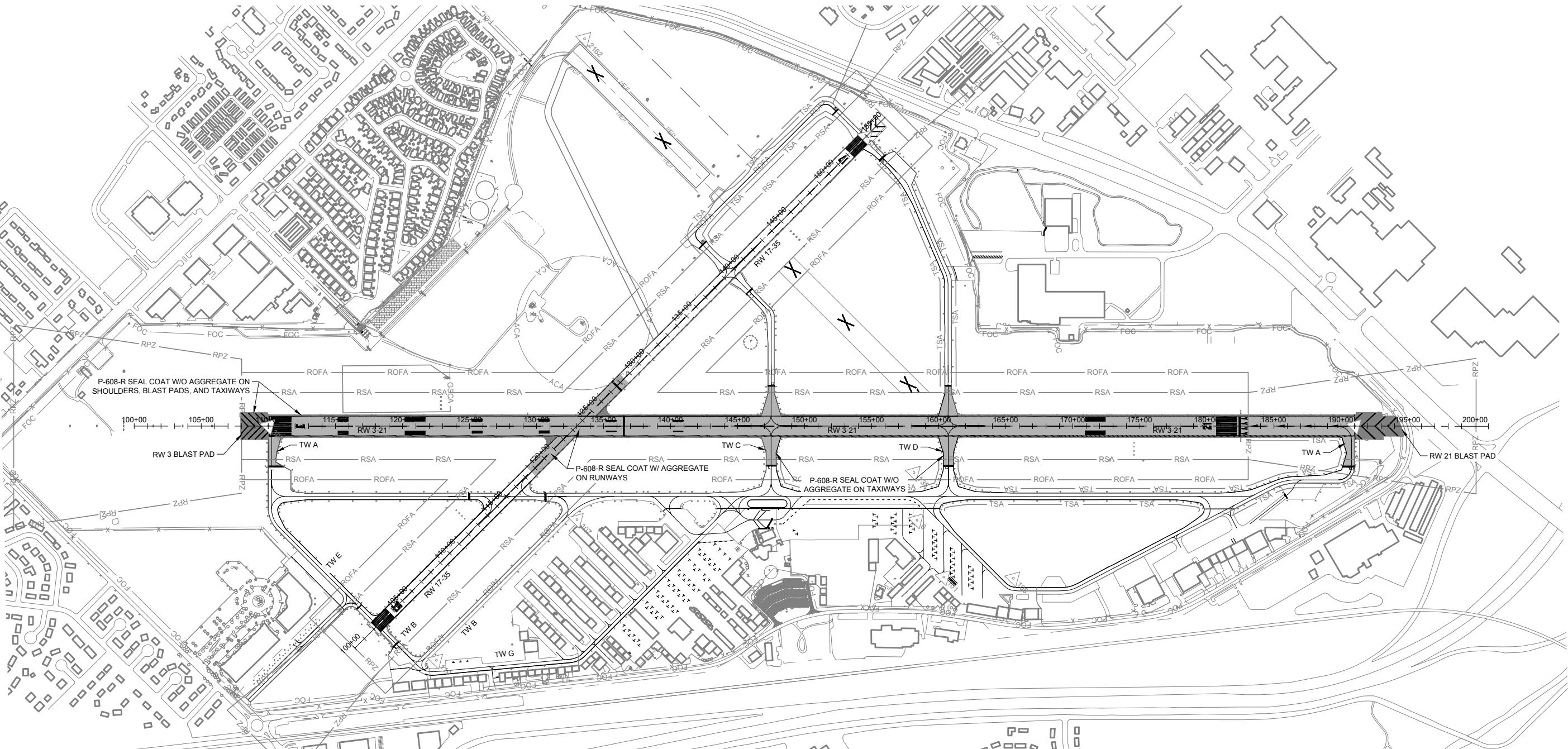
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PROJECT LAYOUT  
 PLAN

SHEET NO. 3 of 22

**G-021**



LEGEND  
 (NEW)  
 EMULSIFIED ASPHALT SEAL COAT  
 WITH AGGREGATE, P-608-R  
 (RUNWAY & TAXIWAY)  
 EMULSIFIED ASPHALT SEAL COAT  
 WITHOUT AGGREGATE, P-608-R  
 (SHOULDER, BLAST PADS)  
 INDICATES A LOCATION WHERE  
 THE MARKING DOES NOT EXIST



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SHEET CONTENTS  
SURVEY CONTROL  
PLAN

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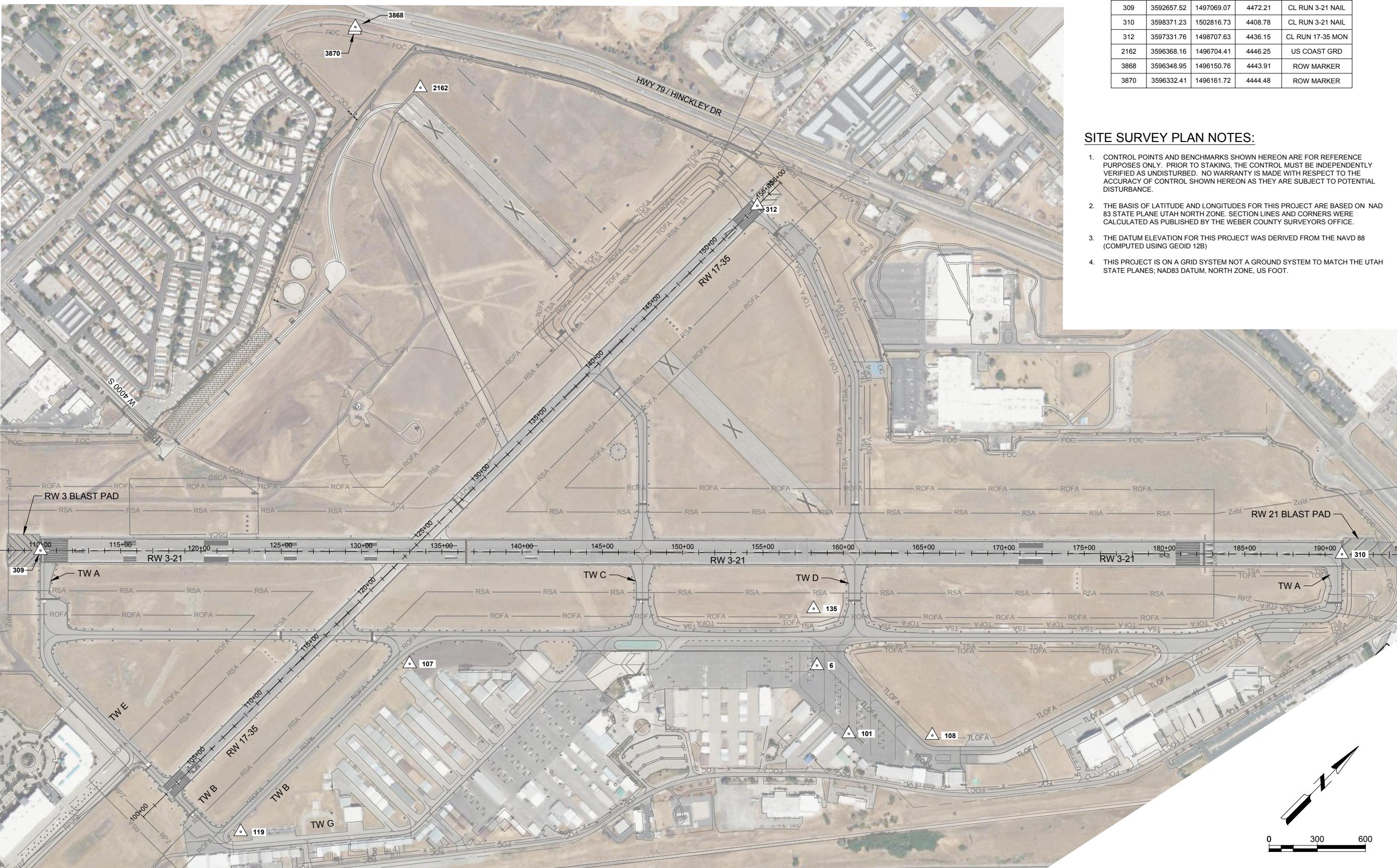
**G-031**

**LEGEND**  
△### CONTROL POINT

POINT TABLE				
POINT #	NORTHING	EASTING	ELEVATION	DESCRIPTION
6	3595570.12	1500995.36	N/A	FND MAG CP-11
101	3595410.11	1501435.63	4432.93	FND MAGNAIL
107	3593784.31	1499193.78	N/A	FND RBARCAP
108	3595769.73	1501810.40	4428.56	FND RBARCAP
119	3592300.40	1499189.75	4453.27	FND RBARCAP
135	3595805.55	1500729.50	4432.61	FND RBAR DIST
309	3592657.52	1497069.07	4472.21	CL RUN 3-21 NAIL
310	3598371.23	1502816.73	4408.78	CL RUN 3-21 NAIL
312	3597331.76	1498707.63	4436.15	CL RUN 17-35 MON
2162	3596368.16	1496704.41	4446.25	US COAST GRD
3868	3596348.95	1496150.76	4443.91	ROW MARKER
3870	3596332.41	1496161.72	4444.48	ROW MARKER

### SITE SURVEY PLAN NOTES:

1. CONTROL POINTS AND BENCHMARKS SHOWN HEREON ARE FOR REFERENCE PURPOSES ONLY. PRIOR TO STAKING, THE CONTROL MUST BE INDEPENDENTLY VERIFIED AS UNDISTURBED. NO WARRANTY IS MADE WITH RESPECT TO THE ACCURACY OF CONTROL SHOWN HEREON AS THEY ARE SUBJECT TO POTENTIAL DISTURBANCE.
2. THE BASIS OF LATITUDE AND LONGITUDES FOR THIS PROJECT ARE BASED ON NAD 83 STATE PLANE UTAH NORTH ZONE. SECTION LINES AND CORNERS WERE CALCULATED AS PUBLISHED BY THE WEBER COUNTY SURVEYORS OFFICE.
3. THE DATUM ELEVATION FOR THIS PROJECT WAS DERIVED FROM THE NAVD 88 (COMPUTED USING GEOID 12B)
4. THIS PROJECT IS ON A GRID SYSTEM NOT A GROUND SYSTEM TO MATCH THE UTAH STATE PLANES; NAD83 DATUM, NORTH ZONE, US FOOT.

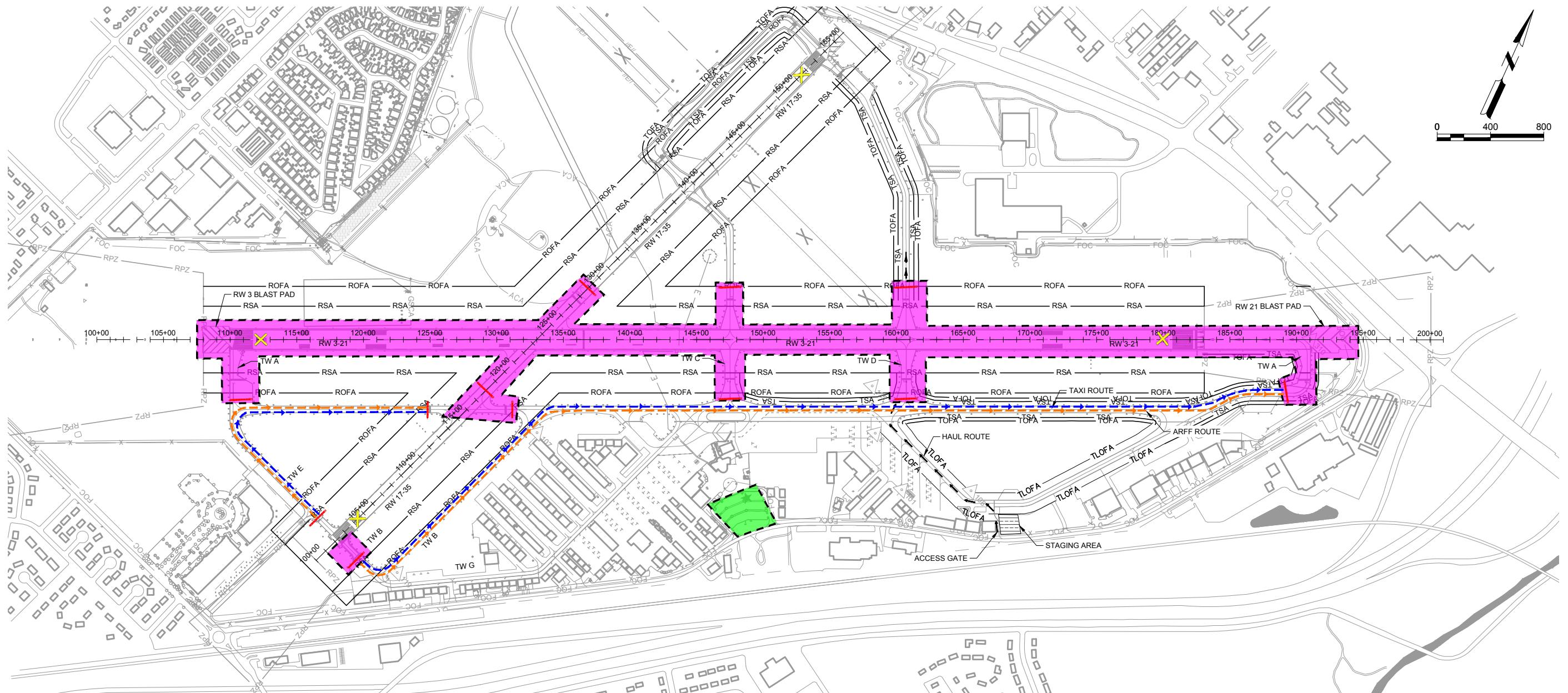




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LEGEND

- CONTRACTOR'S STAGING AREA
- HAUL ROUTE
- LIMIT OF WORK
- ARFF ROUTE
- TAXI ROUTE
- LOW-PROFILE BARRICADES
- FLAG MAN
- GATE
- RUNWAY CLOSURE CROSS
- RSA
- ROFA
- TSA
- TOFA
- RUNWAY SAFETY AREA
- RUNWAY OBJECT FREE AREA
- TAXIWAY SAFETY AREA
- TAXIWAY OBJECT FREE AREA

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CONSTRUCTION  
SAFETY AND PHASING  
PLAN

SHEET NO. 5 of 22

**G-041**



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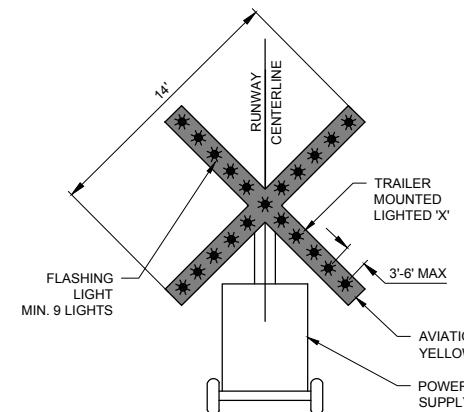
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SHEET CONTENTS  
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**G-042**

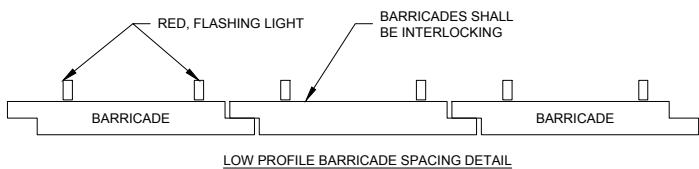


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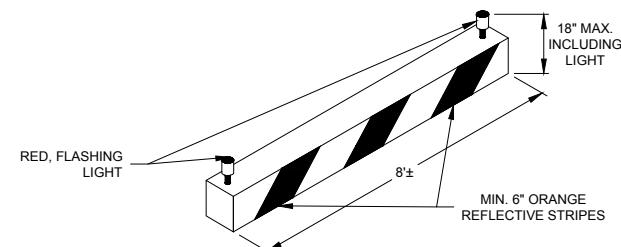
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3. CONTRACTOR SHALL BE RESPONSIBLE FOR ALL MAINTENANCE OF CLOSURE MARKERS FOR THE DURATION OF THE PROJECT. THIS SHALL INCLUDE BUT IS NOT LIMITED TO FUEL, OIL, GREASE, BULB REPLACEMENT, SERVICE AND REPAIR.
4. RUNWAY CLOSURE CROSS SHALL CONFORM TO FAA ADVISORY CIRCULAR 150/5345-55, CURRENT EDITION.
5. THIS ITEM SHALL BE INCIDENTAL TO THE TRAFFIC CONTROL PAY ITEMS AND SHALL INCLUDE FURNISHING, INSTALLING, RELOCATING, MAINTAINING AND REMOVING THE MARKERS AND FOR ALL INCIDENTAL ITEMS TO SUPPLY, OPERATE, AND MAINTAIN THE CROSSES.
6. CROSSES SHALL BE ADEQUATELY SECURED SO AS TO NOT BE AFFECTED BY WIND.

**3 LIGHTED RUNWAY CLOSURE CROSS**

NOT TO SCALE



LOW PROFILE BARRICADE SPACING DETAIL

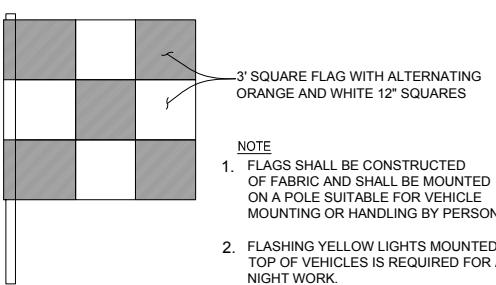


**NOTES:**

1. FLASHERS MUST BE FUNCTIONAL AND DAMAGED BARRICADES SHALL NOT BE USED.
2. BARRICADES TO BE FILLED WITH WATER AND CONSTANTLY MONITORED.
3. LOW PROFILE BARRICADE TO MEET CURRENT EDITION OF FAA AC 150/5370-2 < 18" HEIGHT.
4. UV RESISTANT POLYETHYLENE WATER FILLED, < 8' LENGTH, ORANGE AND WHITE, WITH FLASHING SOLAR AND BATTERY POWERED LIGHTS.

**1 LOW-PROFILE BARRICADE**

NOT TO SCALE



NOTE

1. FLAGS SHALL BE CONSTRUCTED OF FABRIC AND SHALL BE MOUNTED ON A POLE SUITABLE FOR VEHICLE MOUNTING OR HANDLING BY PERSONNEL.
2. FLASHING YELLOW LIGHTS MOUNTED TO TOP OF VEHICLES IS REQUIRED FOR ALL NIGHT WORK.
3. FLAG IS NOT REQUIRED DURING DAYTIME IF VEHICLE UTILIZES FLASHING AMBER LIGHTS.

**2 CONSTRUCTION VEHICLE CHECKERED FLAG DISPLAY**

NOT TO SCALE



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## OGDEN-HINCKLEY AIRPORT RUNWAY 3-21 AND TAXIWAY PAVEMENT MAINTENANCE

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OGDEN, UT 84405

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DESIGNED BY: CJS

DRAWN BY: JAL

CHECKED BY: DAB

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SHEET CONTENTS

CONSTRUCTION

SAFETY AND PHASING

NOTES

SHEET NO. 7 of 22

**G-043**

### GENERAL:

- A. THE CONTRACTOR IS ADVISED THAT ALL EMPLOYEES INSIDE THE AOA FENCE MUST CONFORM WITH ALL AIRFIELD SECURITY REQUIREMENTS. THE CONTRACTOR SHALL CONTACT THE AIRPORT FOR ALL REQUIREMENTS. ANY CONTRACTOR EMPLOYEE NOT CONFORMING TO AIRFIELD OR FAA REQUIREMENTS WILL BE ASKED TO LEAVE THE PROJECT AND NOT RETURN FOR THE REMAINDER OF THE PROJECT.

### SAFETY PLAN COMPLIANCE DOCUMENT

- A. THE CONTRACTOR SHALL PREPARE A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) DESCRIBING HOW THE REQUIREMENTS OF THE CONSTRUCTION SAFETY AND PHASING PLAN WILL BE MET. THE SPCD SHALL INCLUDE A CERTIFICATION STATEMENT BY THE CONTRACTOR THAT:
  - 1. INDICATES FULL UNDERSTANDING OF THE OPERATIONAL SAFETY REQUIREMENTS OF THE CONSTRUCTION SAFETY AND PHASING PLAN AND;
  - 2. ASSERTS NO DEVIATIONS SHALL BE MADE FROM THE APPROVED SAFETY PHASING PLAN AND SPCD UNLESS WRITTEN APPROVAL IS GRANTED BY THE AIRPORT. THE SPCD SHALL FOLLOW THE FORMAT OF THE MOST CURRENT EDITION OF FAA AC 150/5370-2 AND SHALL ADDRESS ITEMS (1)-(18) PER THE REQUIREMENTS OF THE AC.
- B. THE CONTRACTOR SHALL SUBMIT THE SPCD TO THE RESIDENT PROJECT REPRESENTATIVE FOR REVIEW AND APPROVAL A MIN. OF TEN (10) DAYS PRIOR TO THE ANTICIPATED ISSUE OF THE NOTICE TO PROCEED. COPIES OF THE APPROVED CONSTRUCTION SAFETY PHASING PLAN AND SAFETY PLAN COMPLIANCE DOCUMENT SHALL BE MAINTAINED AT THE PROJECT SITE.
- C. DAILY INSPECTIONS BY THE CONTRACTOR AND AIRPORT OPERATOR TO ENSURE COMPLIANCE WITH THE CONSTRUCTION SAFETY PHASING PLANS SHALL BE PERFORMED.
- D. THE CONTRACTOR MUST IMMEDIATELY NOTIFY AIRPORT PERSONNEL OF ANY CONDITIONS ADVERSELY AFFECTING THE SAFE OPERATION ON THE AIRFIELD. CONTACT INFORMATION WILL BE PROVIDED AT THE PRE-CONSTRUCTION MEETING.

### RESTRICTED AREAS / SAFETY AREAS

- A. ALL CONSTRUCTION ACTIVITY SHALL BE COORDINATED THROUGH THE RESIDENT PROJECT REPRESENTATIVE OR ENGINEER AND AIRPORT. CONSTRUCTION ACTIVITY WILL BE AUTHORIZED AFTER NOTAMS HAVE BEEN ISSUED BY THE AIRPORT, AFTER BARRICADES AND LIGHTING PROVISIONS HAVE BEEN IMPLEMENTED BY THE CONTRACTOR, AND AFTER IT HAS BEEN DETERMINED THAT THE HEIGHT OF EQUIPMENT AND MATERIALS ARE BEYOND THE REACH OF THE AIRCRAFT. CONTRACTOR IS RESPONSIBLE TO SUBMIT TO THE FAA A SEPARATE 7460 FOR CONSTRUCTION EQUIPMENT OVER 35 FEET HIGH. CONTRACTOR SHALL SUBMIT ADDITIONAL 7460S A MINIMUM OF 45 WORKING DAYS PRIOR TO START OF CONSTRUCTION.
- B. STOCKPILED MATERIALS AND EQUIPMENT STORAGE IS NOT PERMITTED WITHIN THE SAFETY AREA OR OBJECT FREE AREA OF AN OPERATIONAL RUNWAY OR TAXIWAY. IF THE CONTRACTOR WISHES TO STOCKPILE MATERIALS OR EQUIPMENT ADJACENT TO A RUNWAY/TAXIWAY OBJECT FREE AREA, HE MUST FIRST COORDINATE WITH THE AIRPORT OPERATOR TO ENSURE THAT:
  - 1. APPROPRIATE LIGHTING AND BARRICADES ARE IN PLACE;
  - 2. THE STOCKPILED MATERIALS DO NOT CREATE A WILDLIFE ATTRACTANT OR FOREIGN OBJECT DEBRIS (FOD) HAZARD. EXCAVATION AND RESULTANT STOCKPILES ADJACENT TO OTHER PAVED SURFACES MUST BE APPROPRIATELY MARKED WITH BARRICADES, AS DIRECTED BY THE AIRPORT. DEBRIS SHALL NOT BE DEPOSITED ON ANY PORTION OF AN OPERATIONAL RUNWAY, TAXIWAY OR APRON. SHOULD ANY DEBRIS BE DEPOSITED ON ACTIVE OPERATIONAL AREAS, EITHER INTENTIONALLY OR ACCIDENTALLY, IT SHALL BE REMOVED IMMEDIATELY TO THE SATISFACTION OF THE RPR AND THE AIRPORT.
- C. CLOSING OF RUNWAYS OR TAXIWAYS: WHEN IT BECOMES NECESSARY TO CLOSE OR CHANGE THE STANDARD OPERATIONS OF A RUNWAY OR TAXIWAY, THE CONTRACTOR SHALL, THROUGH THE RPR, NOTIFY THE AIRPORT IN WRITING ONE WEEK IN ADVANCE OF THE PROPOSED CHANGE IN OPERATIONS. SHOULD A RUNWAY OR TAXIWAY BE TEMPORARILY CLOSED, THE CONTRACTOR SHALL COORDINATE WITH AIRPORT OPERATIONS. WHEN CLOSING THE RUNWAY, THE CONTRACTOR SHALL PROVIDE FOUR (4) PORTABLE LIGHTED CLOSURE CROSSES (RCC'S). THE CONTRACTOR IS RESPONSIBLE FOR PLACEMENT AND REMOVAL OF THE RCC'S DURING CONSTRUCTION, AS WELL AS TRANSPORTATION OF THE RCC'S TO AND FROM THE STORAGE AREA WHEN NOT IN USE. ALL OF THESE SAFETY CONTROL DEVICES SHALL REMAIN IN PLACE UNTIL AIRPORT OPENS AFFECTED RUNWAY OR TAXIWAY TO AIRCRAFT OPERATIONS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN THE SAFETY CONTROL DEVICES THROUGHOUT THE DURATION OF THE CLOSURE.
- D. OPEN-FLAME WELDING OR TORCH CUTTING OPERATIONS ARE PROHIBITED UNLESS ADEQUATE FIRE AND SAFETY PRECAUTIONS ARE PROVIDED AS APPROVED BY THE AIRPORT. USE OF ELECTRICAL BLASTING CAPS IS PROHIBITED ON OR WITHIN 1,000 FEET OF THE AIRPORT PROPERTY. FLARE POTS ARE PROHIBITED WITHIN THE AIR OPERATIONS AREA (AOA).
- E. THE CONTRACTOR'S EQUIPMENT IS STRICTLY LIMITED TO THE CONSTRUCTION AREAS DURING CONSTRUCTION AND TO THE CONTRACTOR STAGING/STORAGE AREA DURING NON-CONSTRUCTION PERIODS, EXCEPT AS PROVIDED ON THE PLANS, IN THE CONTRACT SPECIAL PROVISIONS, OR AS MAY BE AUTHORIZED BY THE AIRPORT IN WRITING. MATERIALS SHALL BE STORED IN THE STAGING AREA.

### MARKING, LIGHTING, AND BARRICADES

- A. NO CONSTRUCTION SHALL BEGIN UNTIL ALL BARRICADES, CLOSURE CROSSES, AND SAFETY MEASURES HAVE BEEN PLACED AS SHOWN ON THE CONSTRUCTION SAFETY AND PHASING PLAN SHEETS AND APPROVED BY THE RPR AND/OR THE AIRPORT PERSONNEL. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PLACING PORTABLE BARRICADES AND CLOSURE MARKERS AS SHOWN TO KEEP VEHICLES FROM ENTERING ACTIVE AREAS AND TO KEEP AIRCRAFT FROM TAXIING INTO AREAS UNDER CONSTRUCTION.
- B. ALL CONSTRUCTION EQUIPMENT MUST BE MARKED WITH A 3 FEET BY 3 FEET ORANGE AND WHITE CHECKERED FLAG AND/OR AMBER BEACON. FOR NIGHT CONSTRUCTION, ALL EQUIPMENT MUST BE EQUIPPED WITH AN AMBER BEACON.
- C. BARRICADES SHALL BE LOW PROFILE BARRICADES. BARRICADES SHALL BE ADEQUATELY WEIGHTED SO AS TO WITHSTAND WIND, PROPELLER, AND JET BLAST.

- D. CONTRACTOR MUST PROVIDE AN EMERGENCY CONTACT AVAILABLE 24 HOURS A DAY, 7 DAYS A WEEK FOR MAINTENANCE OF CONSTRUCTION LIGHTING AND BARRICADED, OR ANY OTHER ISSUES THAT INTERFERE WITH THE OPERATIONS AT THE AIRPORT. CONTRACTOR SHALL, ON A DAILY BASIS, COORDINATE WITH RPR OR AIRPORT TO ENSURE ALL REQUIRED AIRFIELD LIGHTING IS FUNCTIONAL, LIGHTED BARRICADES ARE OPERATIONAL, AND TO REVIEW NOTAMS.

- E. TRUCK HAULING SIGNS SHALL BE REMOVED OR COVERED IF TRUCKS ARE NOT HAULING FOR GREATER THAN 24 HOURS AND SHALL BE REINSTALLED OR UNCOVERED ONCE HAULING OPERATIONS RESUME. ALL TEMPORARY TRAFFIC CONTROL SIGNS SHALL BE IN COMPLIANCE WITH MUTCD STANDARDS AND READ AS SHOWN IN THIS PLAN.

### FUEL SUPPORT

- A. ANY TYPE OF FUELING SUPPORT FACILITY OR DEVICE USED TO REFUEL CONSTRUCTION EQUIPMENT IS SUBJECT TO LOCAL FIRE INSPECTION. LOCAL FIRE CODES AND SAFETY STANDARDS SHALL BE MET PRIOR TO COMMENCEMENT OF WORK.
- B. FUELING OF CONSTRUCTION EQUIPMENT SHALL BE COMPLETED ON PAVED SURFACES.

### SWEEEPING / CLEAN-UP

- A. THE CONTRACTOR SHALL HAVE SWEEPING AND VACUUMING CAPABILITIES ON-SITE AT ALL TIMES IN ORDER TO REMOVE DEBRIS FROM ACTIVE OPERATIONAL AREAS (RUNWAYS, TAXIWAYS, APRONS, ENTRANCE ROADS, PARKING LOTS, ETC.). SWEEPER BRISTLES SHALL BE NYLON BRUSH. MATERIAL TRACKED ONTO THESE AREAS MUST BE CONTINUOUSLY REMOVED DURING THE CONSTRUCTION PROJECT. THE CONTRACTOR MUST CONTINUOUSLY VACUUM OR SWEEP JOINT SEAL MATERIAL AND ASPHALT PIECES AS REMOVAL OCCURS.
- B. PRIOR TO OPENING AIRCRAFT MOVEMENT AREAS CLOSED FOR THIS PROJECT, THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE, INCLUDING ADJACENT PAVEMENTS AND HAUL ROUTES, IS CLEAR OF ANY FOREIGN OBJECT DEBRIS (FOD) TO THE SATISFACTION OF THE RPR AND/OR AIRPORT.
- C. UPON COMPLETION OF THIS PROJECT, THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE, INCLUDING ADJACENT PAVEMENTS AND HAUL ROUTES, IS RETURNED TO ORIGINAL CONDITION TO THE SATISFACTION OF THE AIRPORT AND RPR.
- D. THE AIRPORT AND RPR SHALL INSPECT ALL AREAS FOR COMPLIANCE AND SHALL HAVE THE DISCRETION TO ASK FOR CLEANING IF NEEDED, TO THE SATISFACTION OF THE AIRPORT AND RPR.

### HAUL ROUTES, PLANT SITES AND STAGING AREAS

- A. HAUL ROUTES AND ACCESS TO THE CONSTRUCTION SITE(S) WILL BE AS SHOWN ON THE CONSTRUCTION SAFETY AND PHASING PLAN.
- B. ALL VEHICLES ARE TO BE PARKED AND SERVICED IN THE CONTRACTOR STAGING/STORAGE AREA. ACCESS TO THE PROJECT SITE SHALL ONLY BE AT DESIGNATED HAUL ROUTE LOCATIONS.
- C. CONTRACTOR IS RESPONSIBLE FOR RESTORING ALL HAUL ROADS, PLANT SITES AND STAGING AREAS TO ORIGINAL CONDITION. THIS WILL INCLUDE BUT NOT BE LIMITED TO GRADING (FILLING IF NECESSARY) AND SEEDING ALL TURF AREAS. ANY PAVEMENT AREAS USED BY THE CONTRACTOR AS A HAUL ROUTE WHICH ARE DAMAGED WILL BE REPAVED AND/OR REMARKED AS APPROVED BY THE RPR AT THE CONTRACTOR'S EXPENSE.
- D. THE CONTRACTOR IS RESPONSIBLE FOR INSPECTING ALL HAUL ROUTES PRIOR TO THE START OF CONSTRUCTION. THE CONDITION OF EXISTING HAUL ROUTES SHALL BE DOCUMENTED EITHER WITH PICTURES OR VIDEO AND PROVIDED TO THE ENGINEER PRIOR TO ANY HAULING ACTIVITIES. ANTICIPATED COSTS ASSOCIATED WITH DOCUMENTING EXISTING HAUL ROUTE CONDITION, OR RECONSTRUCTING OR RESTORING HAUL ROUTES AND STAGING AREAS SHALL BE INCIDENTAL TO THE PROJECT.

### ACCESS GATE REQUIREMENTS:

- A. THE CONTRACTOR SHALL USE THE GATES SHOWN AND SHALL ADHERE TO ALL SECURITY REQUIREMENTS. ALL CONTRACTOR VEHICLES, INCLUDING DELIVERIES, EMPLOYEES, MATERIALS, INSPECTORS, ETC. WILL ENTER AND EXIT THROUGH THE GATE SHOWN. ALL CONTRACTOR AND SUBCONTRACTOR STAFF SHALL BE BADGED OR ESCORTED PER AIRPORT SECURITY REQUIREMENTS.
- B. THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING THE ACCESS ROAD TO THE SITE. UPON COMPLETION OF THE WORK, ANY ACCESS ROADS WILL BE RESTORED TO THE ORIGINAL CONDITION.

### UNDERGROUND ELECTRICAL CIRCUITS & UTILITIES

- A. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTION OF EXISTING AIRFIELD UTILITIES, AND ELECTRICAL CIRCUITS, WHETHER OWNED BY THE AIRPORT OR OTHER AGENCIES OPERATING AT THE AIRPORT. LOCATIONS OF EXISTING UTILITIES SHOWN IN THE PLANS ARE BASED ON AVAILABLE AS-BUILT DATA AND ARE APPROXIMATE ONLY. THE CONTRACTOR SHALL COORDINATE THE LOCATION OF ANY CROSSING OF UNDERGROUND ELECTRICAL CIRCUITS AND UTILITIES WITH THE AIRPORT, AND FAA FACILITIES PRIOR TO THE START OF ANY CONSTRUCTION ACTIVITIES. PRIOR TO CONSTRUCTION, IT IS THE CONTRACTOR'S RESPONSIBILITY TO ENSURE THAT ALL UTILITIES ARE LOCATED. FOR UTAH 811, CALL 1-801-208-2100 OR 811.

### WILDLIFE MANAGEMENT

- A. CONTRACTOR MUST AVOID CREATING WILDLIFE HAZARDS ON THE AIRPORT SUCH AS TRASH, STANDING WATER, TALL GRASS AND SEEDS. SEEDING SHALL COMPLY WITH THE SPECIFICATIONS.

### MEETINGS AND CORRESPONDENCE

- A. THE AIRPORT SHALL ISSUE ALL NOTAMS. THE CONTRACTOR MUST NOTIFY THE RPR OR AIRPORT A MINIMUM OF 48 HOURS PRIOR TO NOTAM TAKING EFFECT.
- B. CONTRACTOR IS REQUIRED TO ATTEND A PRE-CONSTRUCTION MEETING ALONG WITH WEEKLY COORDINATION MEETINGS AT WHICH SAFETY ISSUES AND PROJECT PROGRESS WILL BE DISCUSSED.

### AIRPORT OPERATIONS

- A. THE AIRPORT WILL MAINTAIN NORMAL HOURS OF OPERATIONS FOR THE DURATION OF THIS PROJECT. TENANT ACCESS TO HANGAR FACILITIES SHALL NOT BE IMPEDED AT ANY TIME.

### ENFORCEMENT

- A. VIOLATIONS TO THE AIRPORT OPERATIONS OR THESE RULES AND REGULATIONS, DEPENDING UPON THE SEVERITY OF THE VIOLATION, MAY RESULT IN ONE OR MORE OF THE FOLLOWING:
  - 1. A VERBAL AND/OR WRITTEN WARNING
  - 2. THE CONTRACT WORK BEING STOPPED UNTIL CORRECTIVE MEASURES ARE TAKEN TO PRECLUDE A RECURRENT OF THE VIOLATIONS. NO TIME EXTENSION WILL BE ISSUED DUE TO A VIOLATION.
  - 3. REMOVAL FROM THE PROJECT.
  - 4. FINES ISSUED BY THE AIRPORT OR BY TSA.

### OPERATIONS AND PHASING

- A. EXACT LIMITS OF CONSTRUCTION WILL BE ESTABLISHED BY THE CONTRACTOR AND VERIFIED BY THE RPR PRIOR TO THE CONTRACTOR BEGINNING WORK IN ANY AREA. THE CONTRACTOR SHALL BE RESPONSIBLE FOR INFORMING ALL EMPLOYEES OF THESE LIMITS.

### BADGING:

- A. ALL EMPLOYEES WORKING INSIDE THE AOA FENCE MUST ACQUIRE AN AOA BADGE PRIOR TO CONSTRUCTION. CONTRACTOR TO CONFIRM BADGING REQUIREMENTS AT FLYOGDEN.COM THE AIRPORT WILL PROVIDE BADGE APPLICATIONS TO THE CONTRACTOR AT THE PRECONSTRUCTION MEETING.



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## OGDEN-HINCKLEY AIRPORT RUNWAY 3-21 AND TAXIWAY PAVEMENT MAINTENANCE

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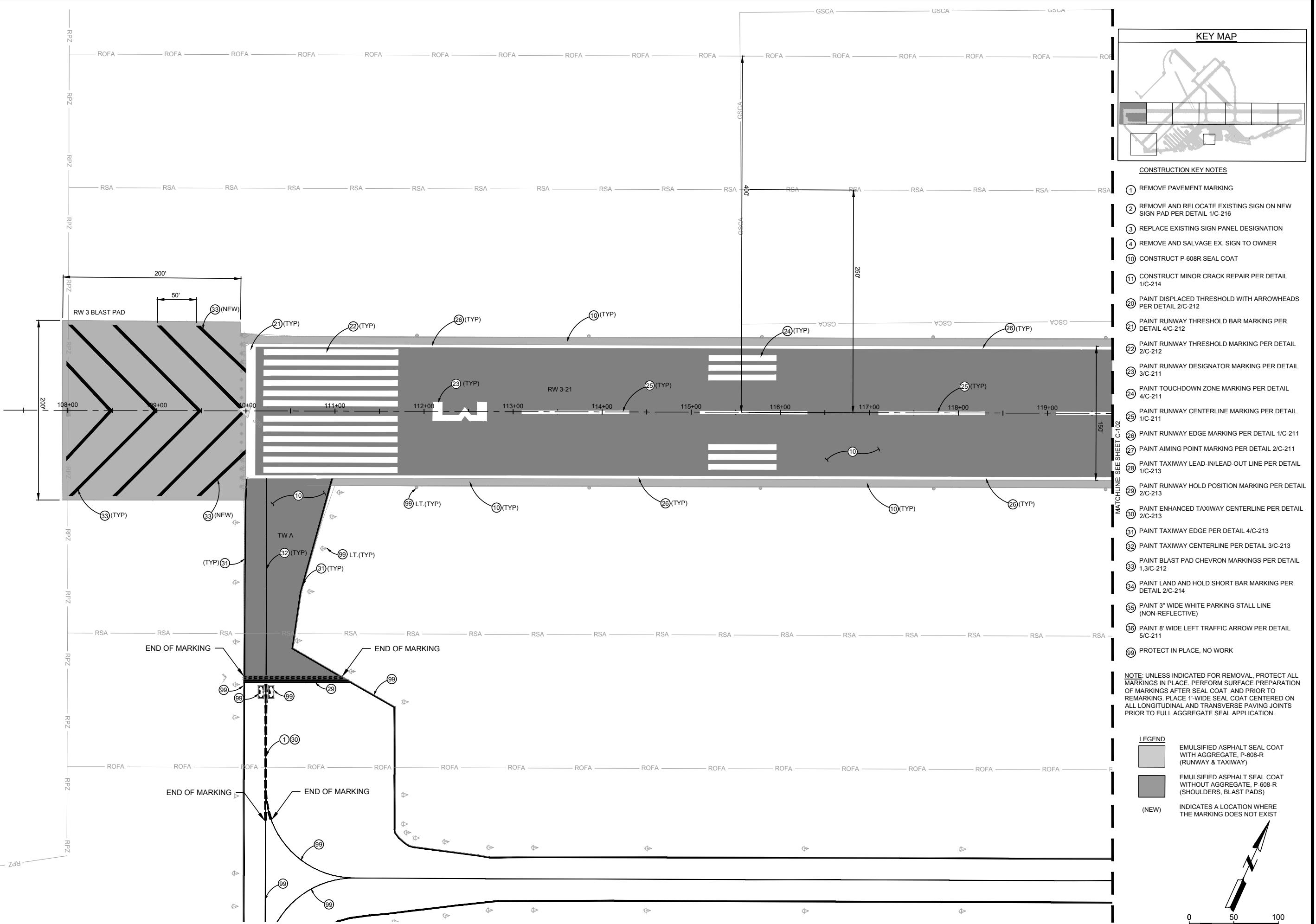
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CONSTRUCTION PLAN

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## OGDEN-HINCKLEY AIRPORT RUNWAY 3-21 AND TAXIWAY PAVEMENT MAINTENANCE

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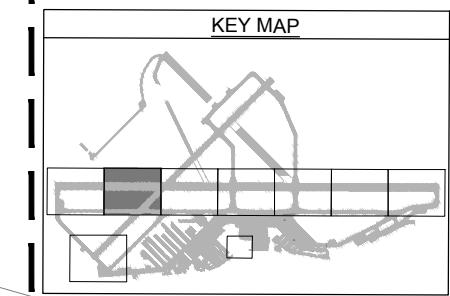
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SHEET CONTENTS  
CONSTRUCTION PLAN

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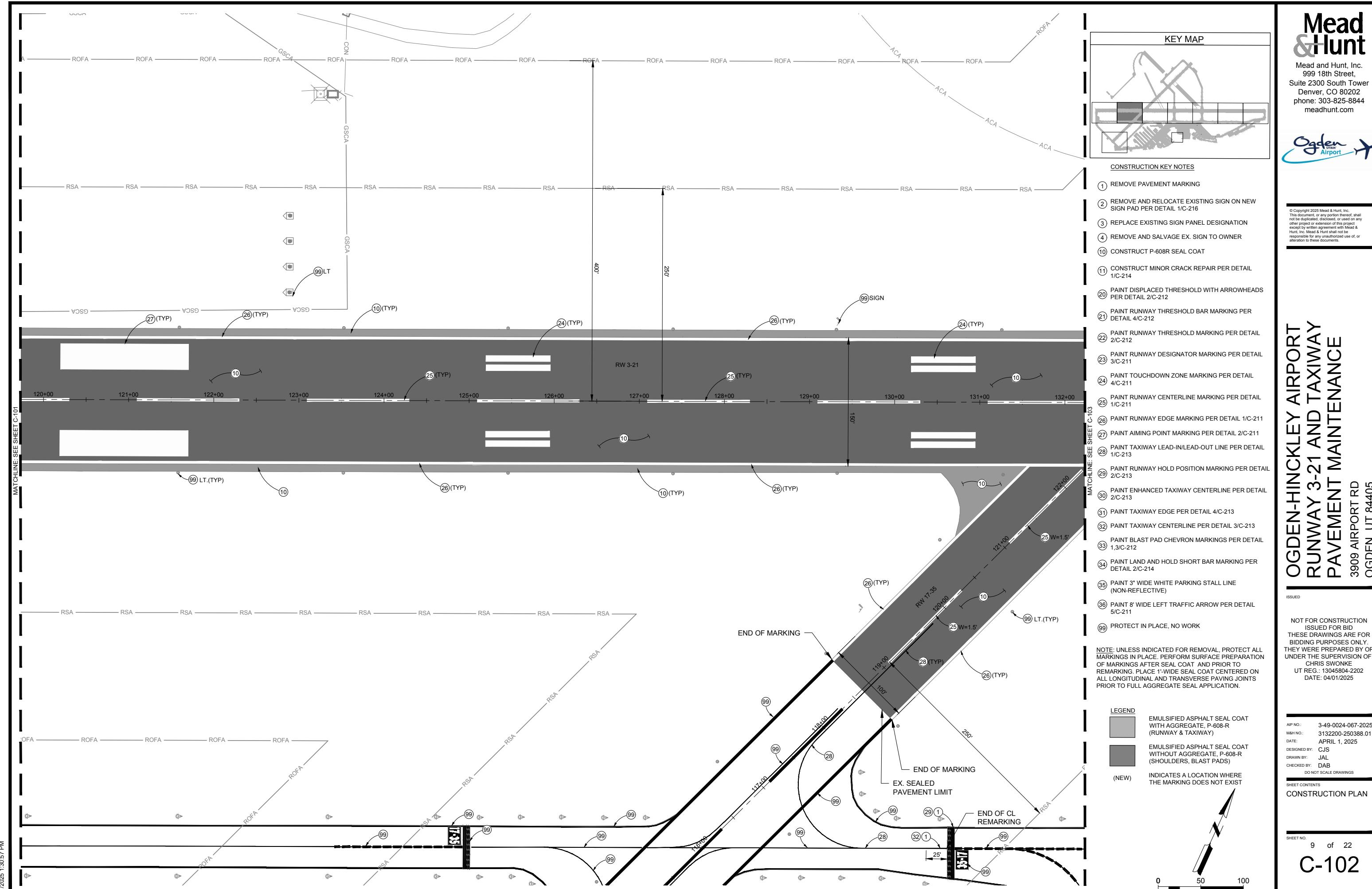


### CONSTRUCTION KEY NOTES

- ① REMOVE PAVEMENT MARKING
- ② REMOVE AND RELOCATE EXISTING SIGN ON NEW SIGN PAD PER DETAIL 1/C-216
- ③ REPLACE EXISTING SIGN PANEL DESIGNATION
- ④ REMOVE AND SALVAGE EX. SIGN TO OWNER
- ⑩ CONSTRUCT P-608R SEAL COAT
- ⑪ CONSTRUCT MINOR CRACK REPAIR PER DETAIL 1/C-214
- ⑯ PAINT DISPLACED THRESHOLD WITH ARROWHEADS PER DETAIL 2/C-212
- ⑯ PAINT RUNWAY THRESHOLD BAR MARKING PER DETAIL 4/C-212
- ⑯ PAINT RUNWAY THRESHOLD MARKING PER DETAIL 2/C-212
- ⑯ PAINT RUNWAY DESIGNATOR MARKING PER DETAIL 3/C-211
- ⑯ PAINT TOUCHDOWN ZONE MARKING PER DETAIL 4/C-211
- ⑯ PAINT RUNWAY CENTERLINE MARKING PER DETAIL 1/C-211
- ⑯ PAINT RUNWAY EDGE MARKING PER DETAIL 1/C-211
- ⑯ PAINT AIMING POINT MARKING PER DETAIL 2/C-211
- ⑯ PAINT TAXIWAY LEAD-IN/LEAD-OUT LINE PER DETAIL 1/C-213
- ⑯ PAINT RUNWAY HOLD POSITION MARKING PER DETAIL 2/C-213
- ⑯ PAINT ENHANCED TAXIWAY CENTERLINE PER DETAIL 2/C-213
- ⑯ PAINT TAXIWAY EDGE PER DETAIL 4/C-213
- ⑯ PAINT TAXIWAY CENTERLINE PER DETAIL 3/C-213
- ⑯ PAINT BLAST PAD CHEVRON MARKINGS PER DETAIL 1.3/C-212
- ⑯ PAINT LAND AND HOLD SHORT BAR MARKING PER DETAIL 2/C-214
- ⑯ PAINT 3" WIDE WHITE PARKING STALL LINE (NON-REFLECTIVE)
- ⑯ PAINT 8" WIDE LEFT TRAFFIC ARROW PER DETAIL 5/C-211
- ⑯ PROTECT IN PLACE, NO WORK

NOTE: UNLESS INDICATED FOR REMOVAL, PROTECT ALL MARKINGS IN PLACE. PERFORM SURFACE PREPARATION OF MARKINGS AFTER SEAL COAT AND PRIOR TO REMARKING. PLACE 1"-WIDE SEAL COAT CENTERED ON ALL LONGITUDINAL AND TRANSVERSE PAVING JOINTS PRIOR TO FULL AGGREGATE SEAL APPLICATION.

**LEGEND**  
  
  
  
 INDICATES A LOCATION WHERE THE MARKING DOES NOT EXIST





OGDEN-HINCKLEY AIRPORT  
RUNWAY 3-21 AND TAXIWAY  
PAVEMENT MAINTENANCE  
3909 AIRPORT RD  
OGDEN, UT 84405

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## CONSTRUCTION PLAN

#### CONSTRUCTION PERIOD

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## KEY MAP

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**OGDEN-HINCKLEY AIRPORT  
RUNWAY 3-21 AND TAXIWAY  
PAVEMENT MAINTENANCE**  
3909 AIRPORT RD  
OGDEN, UT 84405

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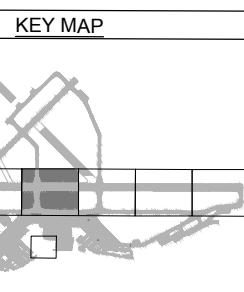
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CONSTRUCTION PLAN

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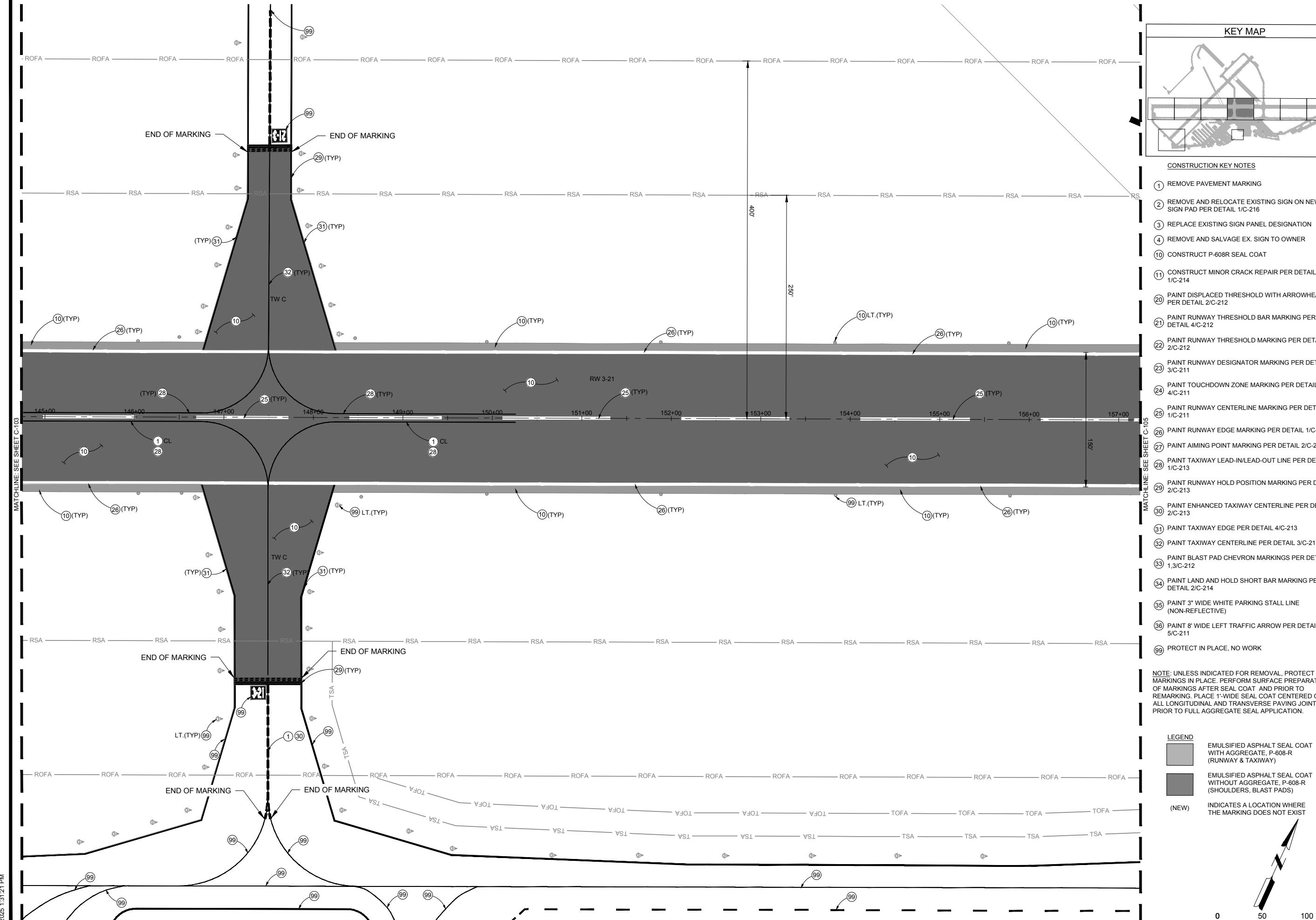


**CONSTRUCTION KEY NOTES**

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LEGEND	EMULSIFIED ASPHALT SEAL COAT WITH AGGREGATE, P-608-R (RUNWAY & TAXIWAY)
	EMULSIFIED ASPHALT SEAL COAT WITHOUT AGGREGATE, P-608-R (SHOULDERS, BLAST PADS)
(NEW)	INDICATES A LOCATION WHERE THE MARKING DOES NOT EXIST



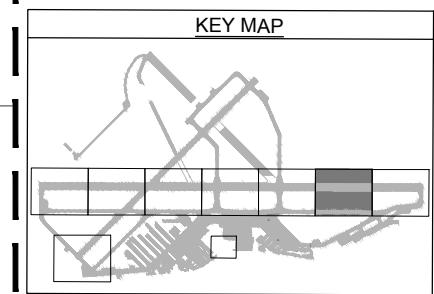




## OGDEN-HINCKLEY AIRPORT RUNWAY 3-21 AND TAXIWAY PAVEMENT MAINTENANCE

3909 AIRPORT RD  
OGDEN, UT 84405

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### CONSTRUCTION KEY NOTES

- ① REMOVE PAVEMENT MARKING
- ② REMOVE AND RELOCATE EXISTING SIGN ON NEW SIGN PAD PER DETAIL 1/C-216
- ③ REPLACE EXISTING SIGN PANEL DESIGNATION
- ④ REMOVE AND SALVAGE EX. SIGN TO OWNER
- ⑩ CONSTRUCT P-608R SEAL COAT
- ⑪ CONSTRUCT MINOR CRACK REPAIR PER DETAIL 1/C-214
- ⑯ PAINT DISPLACED THRESHOLD WITH ARROWHEADS PER DETAIL 2/C-212
- ⑰ PAINT RUNWAY THRESHOLD BAR MARKING PER DETAIL 4/C-212
- ⑱ PAINT RUNWAY THRESHOLD MARKING PER DETAIL 2/C-212
- ⑲ PAINT RUNWAY DESIGNATOR MARKING PER DETAIL 3/C-211
- ⑳ PAINT TOUCHDOWN ZONE MARKING PER DETAIL 4/C-211
- ㉑ PAINT RUNWAY CENTERLINE MARKING PER DETAIL 1/C-211
- ㉒ PAINT RUNWAY EDGE MARKING PER DETAIL 1/C-211
- ㉓ PAINT AIMING POINT MARKING PER DETAIL 2/C-211
- ㉔ PAINT TAXIWAY LEAD-IN/LEAD-OUT LINE PER DETAIL 1/C-213
- ㉕ PAINT RUNWAY HOLD POSITION MARKING PER DETAIL 2/C-213
- ㉖ PAINT ENHANCED TAXIWAY CENTERLINE PER DETAIL 2/C-213
- ㉗ PAINT TAXIWAY EDGE PER DETAIL 4/C-213
- ㉘ PAINT TAXIWAY CENTERLINE PER DETAIL 3/C-213
- ㉙ PAINT BLAST PAD CHEVRON MARKINGS PER DETAIL 1.3/C-212
- ㉚ PAINT LAND AND HOLD SHORT BAR MARKING PER DETAIL 2/C-214
- ㉛ PAINT 3" WIDE WHITE PARKING STALL LINE (NON-REFLECTIVE)
- ㉜ PAINT 8" WIDE LEFT TRAFFIC ARROW PER DETAIL 5/C-211
- ㉖ PROTECT IN PLACE, NO WORK

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**C-106**

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**OGDEN-HINCKLEY AIRPORT  
RUNWAY 3-21 AND TAXIWAY  
PAVEMENT MAINTENANCE**  
3909 AIRPORT RD  
OGDEN, UT 84405

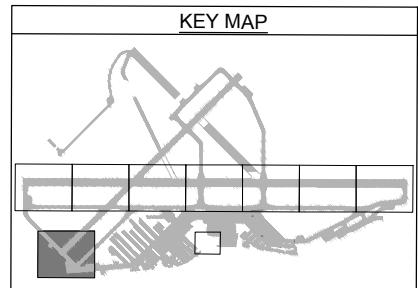
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CONSTRUCTION PLAN

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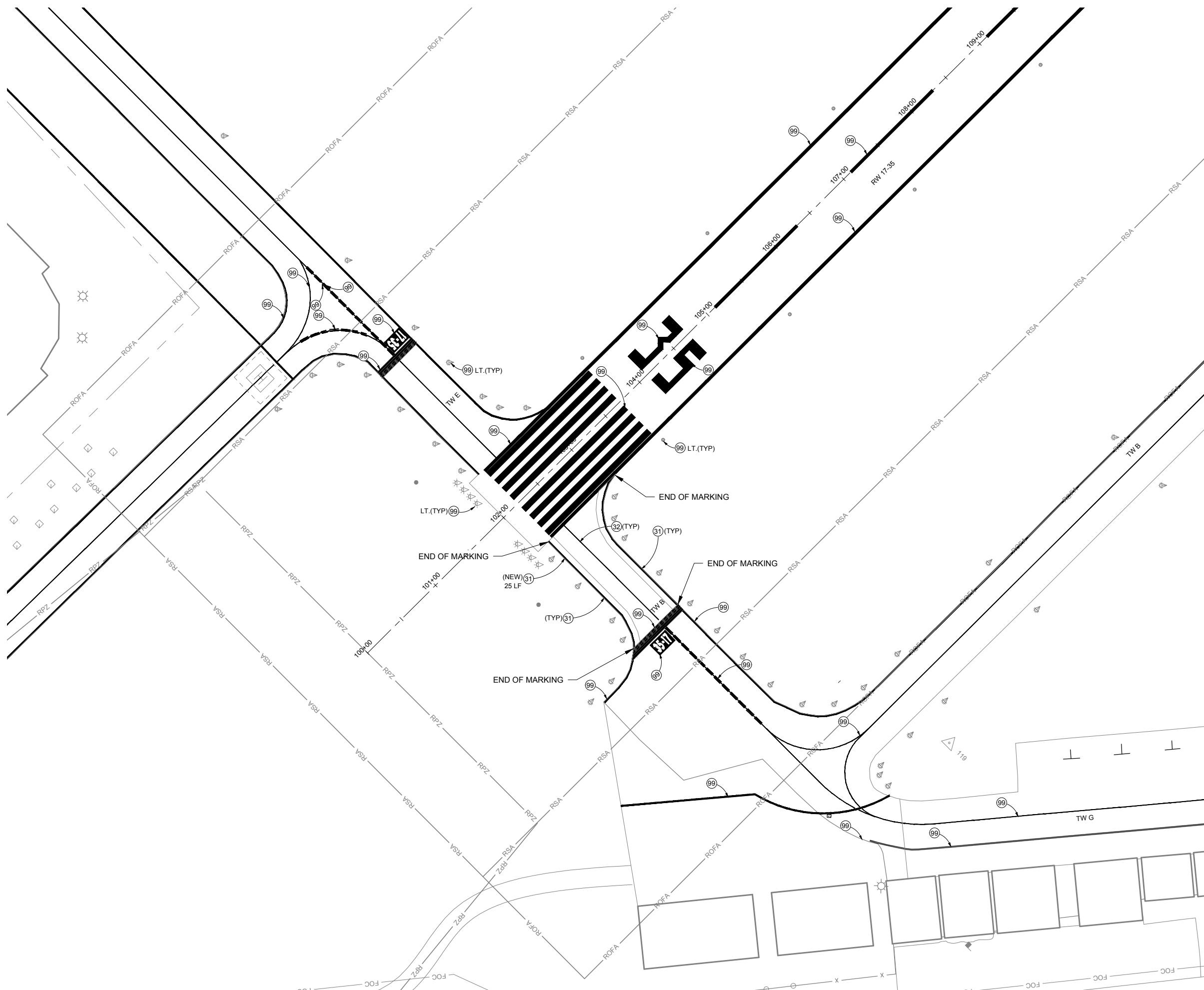
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- ㉒ PAINT RUNWAY THRESHOLD MARKING PER DETAIL 2/C-212
- ㉓ PAINT RUNWAY DESIGNATOR MARKING PER DETAIL 3/C-211
- ㉔ PAINT TOUCHDOWN ZONE MARKING PER DETAIL 4/C-211
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- ㉛ PAINT TAXIWAY EDGE PER DETAIL 4/C-213
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- ㉟ PAINT 8' WIDE LEFT TRAFFIC ARROW PER DETAIL 5/C-211
- ㉞ PROTECT IN PLACE, NO WORK

NOTE: UNLESS INDICATED FOR REMOVAL, PROTECT ALL  
MARKINGS IN PLACE. PERFORM SURFACE PREPARATION  
OF MARKINGS AFTER SEAL COAT AND PRIOR TO  
REMARKING. PLACE 1"-WIDE SEAL COAT CENTERED ON  
ALL LONGITUDINAL AND TRANSVERSE PAVING JOINTS  
PRIOR TO FULL AGGREGATE SEAL APPLICATION.

LEGEND

- EMULSIFIED ASPHALT SEAL COAT  
WITH AGGREGATE, P-608-R  
(RUNWAY & TAXIWAY)
- EMULSIFIED ASPHALT SEAL COAT  
WITHOUT AGGREGATE, P-608-R  
(SHOULDERS, BLAST PADS)
- (NEW) INDICATES A LOCATION WHERE  
THE MARKING DOES NOT EXIST





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RUNWAY 3-21 AND TAXIWAY  
PAVEMENT MAINTENANCE**  
3909 AIRPORT RD  
OGDEN, UT 84405

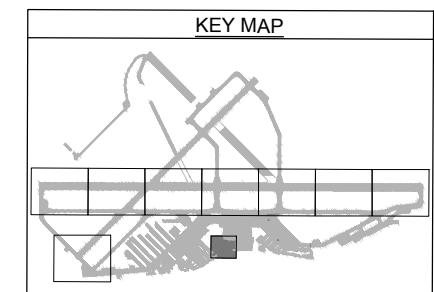
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SHEET CONTENTS  
PARKING LOT  
CONSTRUCTION PLAN

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CONSTRUCTION KEY NOTES

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- ② REMOVE AND RELOCATE EXISTING SIGN ON NEW SIGN PAD PER DETAIL 1/C-216
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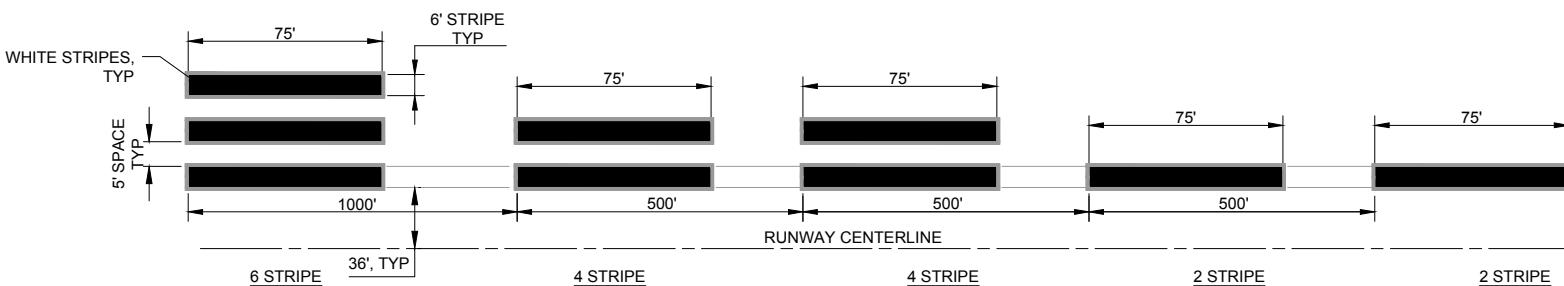
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EMULSIFIED ASPHALT SEAL COAT  
WITH AGGREGATE, P-608-R  
(RUNWAY & TAXIWAY)

EMULSIFIED ASPHALT SEAL COAT  
WITHOUT AGGREGATE, P-608-R  
(SHOULDERS, BLAST PADS)

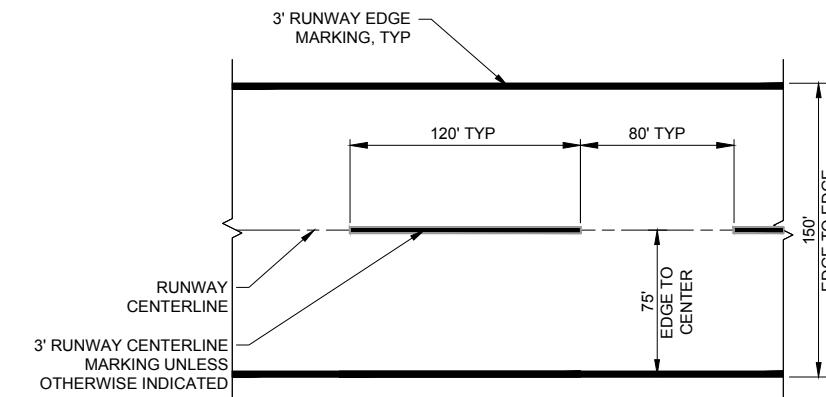
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THE MARKING DOES NOT EXIST

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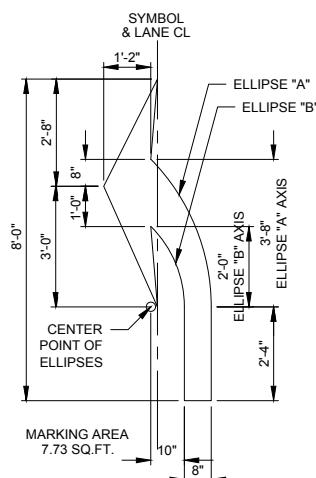
**NOTE:**  
1. RUNWAY TOUCHDOWN ZONE MARKINGS ARE WHITE WITH 6" BLACK OUTLINE. DIMENSIONS SHOWN EXCLUDE THE BLACK OUTLINE.

**4 RUNWAY TOUCHDOWN ZONE MARKING**  
NO SCALE



**NOTES:**  
1. RUNWAY CENTERLINE MARKINGS ARE WHITE WITH 6" BLACK OUTLINE. DIMENSIONS SHOWN EXCLUDE THE OUTLINE.

**1 RUNWAY CENTERLINE AND EDGE MARKING**  
NO SCALE

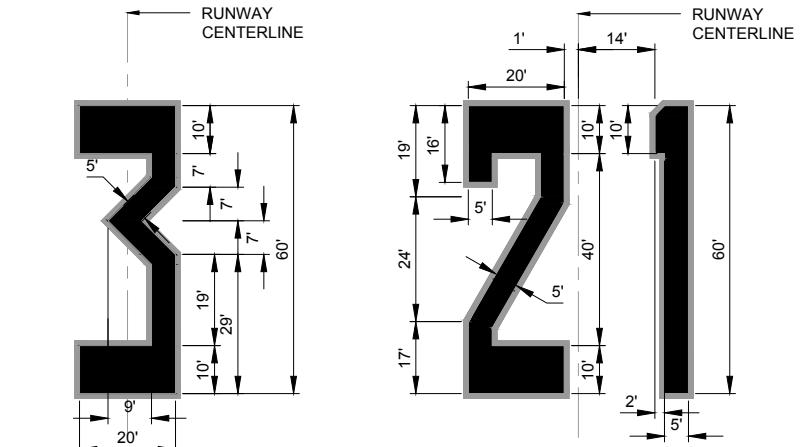


**NOTE:**  
1. RUNWAY AIMING POINT MARKINGS ARE WHITE WITH 6" BLACK OUTLINE. DIMENSIONS SHOWN EXCLUDE THE BLACK OUTLINE.

**2 RUNWAY AIMING POINT MARKING**  
NO SCALE



**NOTE:**  
1. ALL TRAFFIC ARROWS WILL BE PAINTED SOLID WHITE.



**NOTES:**  
1. RUNWAY LANDING DESIGNATOR MARKINGS ARE WHITE WITH 6" BLACK OUTLINE. DIMENSIONS SHOWN EXCLUDE THE BLACK OUTLINE.  
2. ALL CHARACTERS HAVE THESE CHARACTERISTICS: 60 FT HIGH, 20 FT WIDE, VERTICAL STROKE OF 5 FT, HORIZONTAL STROKE OF 10 FT AND DIAGONAL STROKE OF 5 FT.

**3 RUNWAY 3-21 LANDING DESIGNATOR MARKING**  
NO SCALE

**OGDEN-HINCKLEY AIRPORT  
RUNWAY 3-21 AND TAXIWAY  
PAVEMENT MAINTENANCE**  
3909 AIRPORT RD  
OGDEN, UT 84405

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**C-211**



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**OGDEN-HINCKLEY AIRPORT  
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PAVEMENT MAINTENANCE**  
3909 AIRPORT RD  
OGDEN, UT 84405

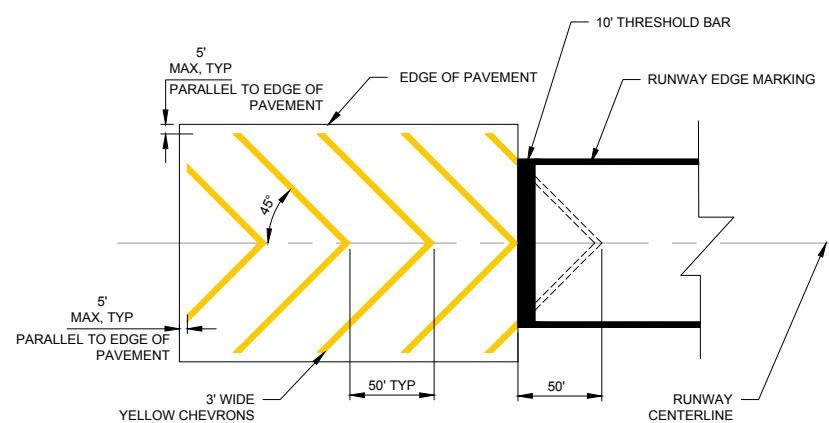
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CONSTRUCTION  
DETAILS

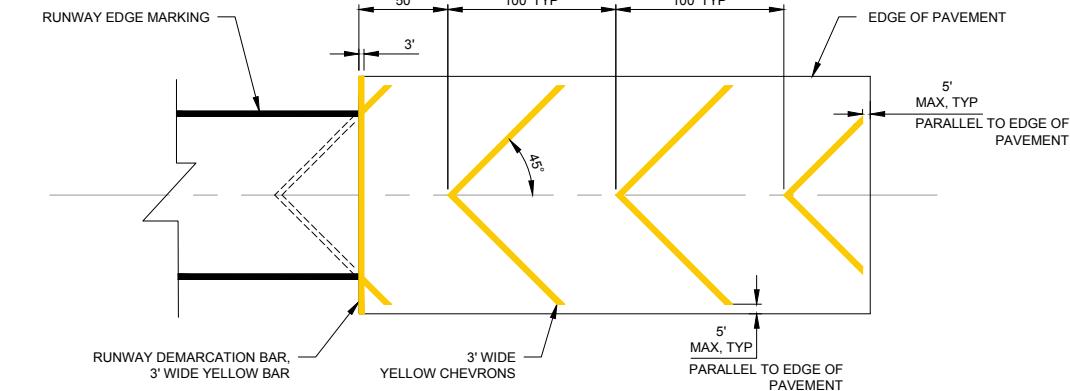
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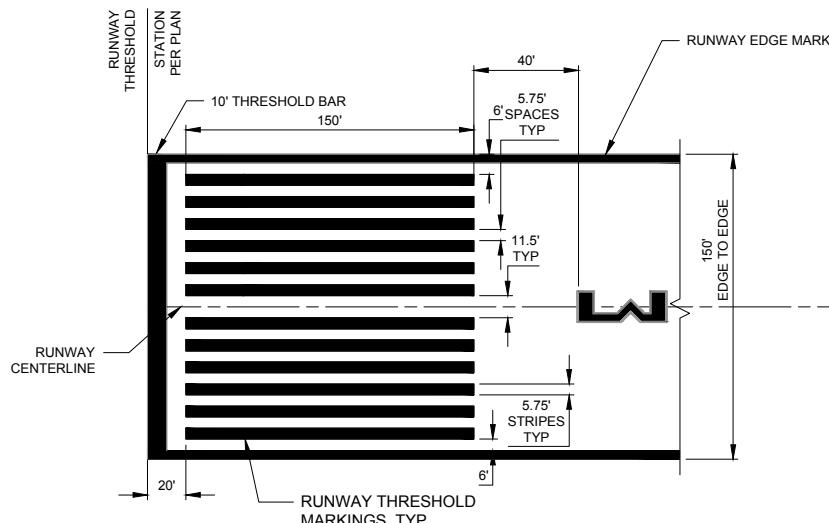
NOTE:  
1. THE FIRST FULL CHEVRON STARTS AT THE INDEX POINT (INTERSECTION OF RUNWAY CENTERLINE AND RUNWAY THRESHOLD TOE).  
2. GLASS BEADS SHALL NOT BE APPLIED TO CHEVRONS.  
3. RUNWAY CHEVRON MARKINGS ARE YELLOW WITH 6" BLACK OUTLINE. DIMENSIONS SHOWN EXCLUDE THE BLACK OUTLINE.

**3 RUNWAY 3 BLAST PAD MARKING**  
NO SCALE



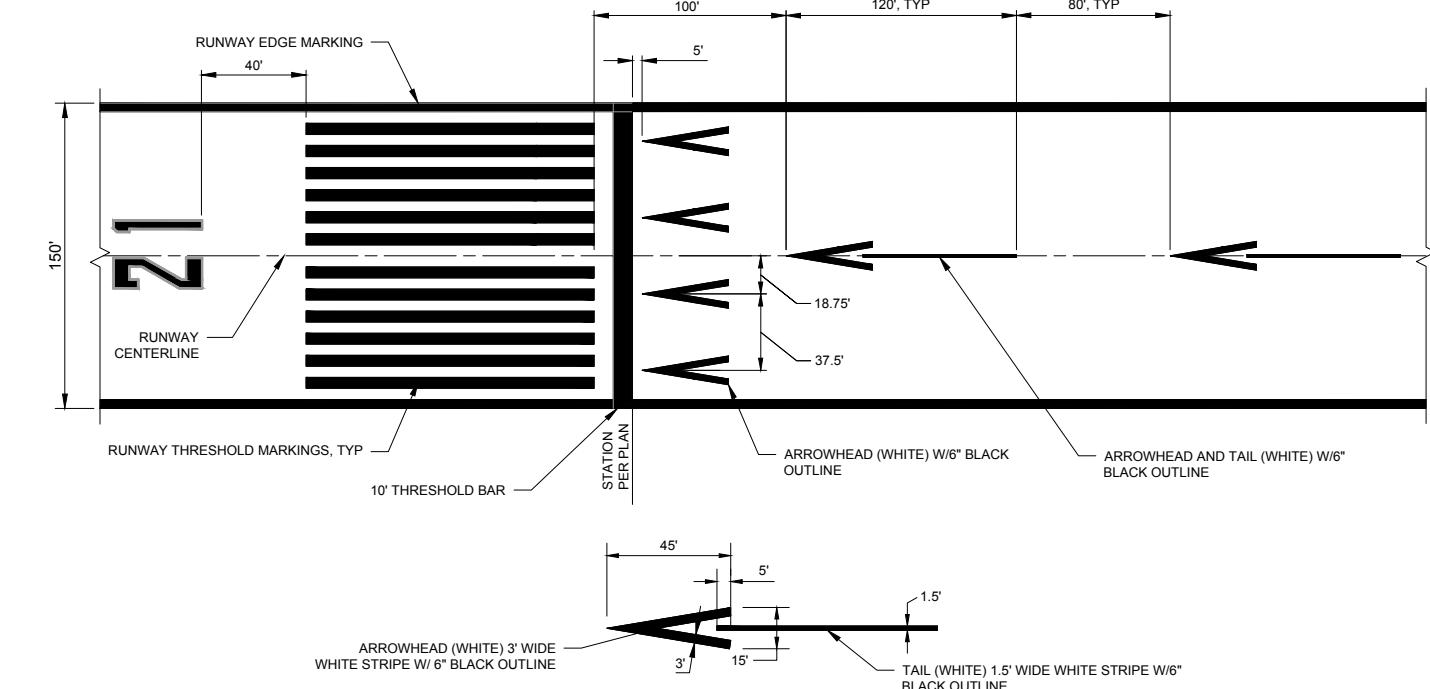
NOTE:  
1. THE FIRST FULL CHEVRON STARTS AT 50' FROM THE START OF THE BLAST PAD.  
2. RUNWAY DEMARCTION BAR DOES NOT HAVE BLACK BORDER.  
3. GLASS BEADS SHALL NOT BE APPLIED TO CHEVRONS.  
4. RUNWAY CHEVRON MARKINGS ARE YELLOW WITH 6" BLACK OUTLINE. DIMENSIONS SHOWN EXCLUDE THE BLACK OUTLINE.

**1 RUNWAY 21 BLAST PAD MARKING**  
NO SCALE



NOTE:  
1. RUNWAY THRESHOLD AND THRESHOLD BAR MARKINGS ARE WHITE WITH 6" BLACK OUTLINE. DIMENSIONS SHOWN EXCLUDE THE BLACK OUTLINE.

**4 RUNWAY THRESHOLD MARKING  
AND THRESHOLD BAR MARKING**  
NO SCALE



NOTE:  
1. ALL RUNWAY DISPLACED THRESHOLD MARKINGS ARE WHITE W/ 6" BLACK OUTLINE. DIMENSIONS SHOWN EXCLUDE THE OUTLINE.

**2 RUNWAY DISPLACED THRESHOLD MARKING**  
NO SCALE



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A diagram showing a horizontal yellow stripe on a grey background. The stripe is 6 inches wide and 6 inches long. It is outlined with a black border. To the left, a vertical dimension line indicates the height of the stripe, with three 6-inch segments stacked vertically. Arrows point from the text labels to the corresponding parts of the stripe.

6"

6"

6"

BLACK OUTLINE

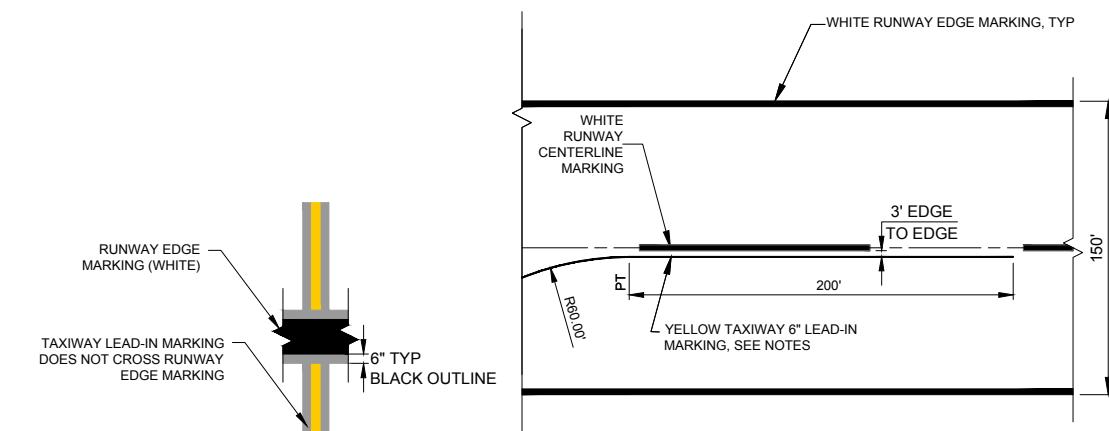
YELLOW PAINT  
W/ 6" BLACK OUTLINE

**NOTE:**

1. TAXIWAY CENTERLINE MARKINGS ARE YELLOW WITH 6" BLACK OUTLINE.
2. ALL YELLOW PAVEMENT MARKINGS CONTAIN GLASS BEADS.
3. BLACK MARKINGS DO NOT CONTAIN GLASS BEADS.

## 3 TAXIWAY CENTERLINE MARKING

3 NO SCAN



NOTE:

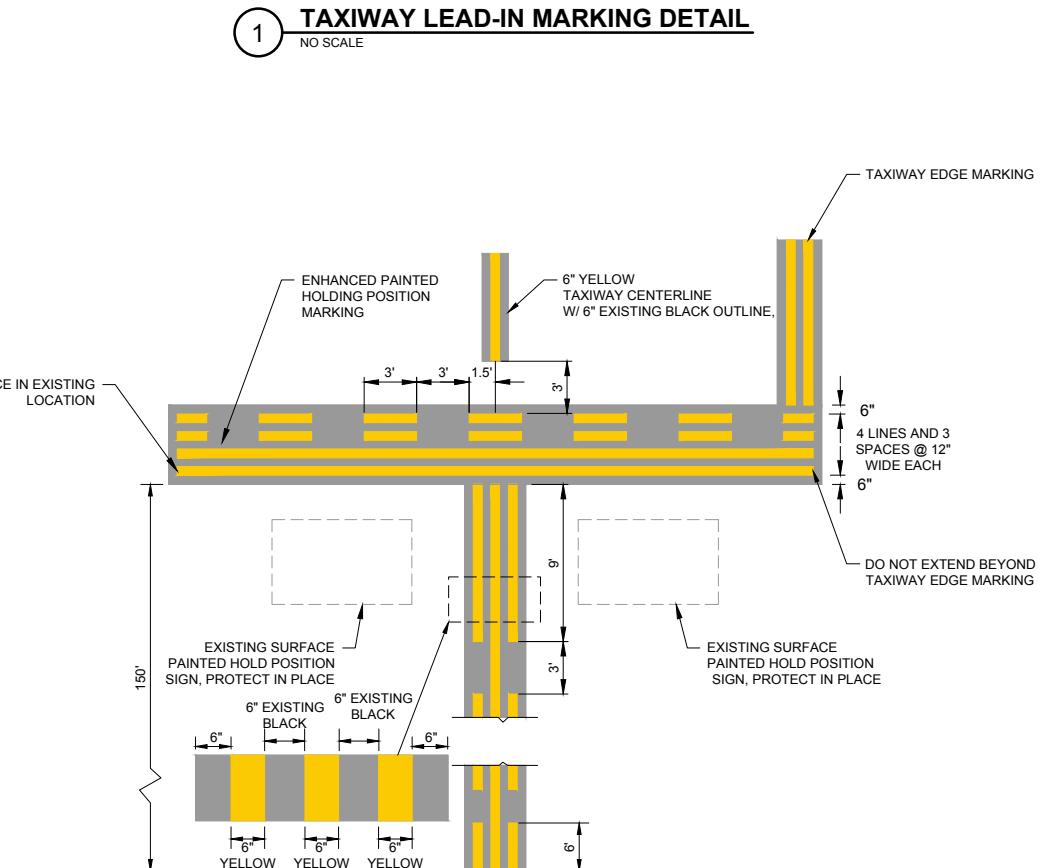
1. TAXIWAY LEAD-IN MARKINGS ARE YELLOW W/6" BLACK OUTLINE. DIMENSIONS SHOWN EXCLUDE THE BLACK OUTLINE.
2. ALL WHITE AND YELLOW MARKINGS CONTAIN GLASS BEADS.
3. BLACK MARKINGS DO NOT CONTAIN GLASS BEADS.

The diagram illustrates a continuous yellow centerline on a taxiway. The centerline is 6 inches thick and 15 feet long. It is marked with a 6-inch black outline. The total width of the centerline is 18 inches. The diagram also shows the shoulder and taxiway pavements, with dimensions of 6 inches for the shoulder and 25 feet for the taxiway pavements. The centerline is labeled as CONTINUOUS.

NOTE:  
1. ALL YELLOW PAVEMENT MARKINGS CONTAIN GLASS BEADS.  
2. BLACK MARKINGS DO NOT CONTAIN GLASS BEADS.

## 1 TAXIWAY EDGE MARKING

4 NO S



**NOTE:**

1. TAXIWAY HOLD POSITION AND CENTERLINE MARKINGS ARE YELLOW W/6" EXISTING BLACK OUTLINE. DIMENSIONS SHOWN EXCLUDE THE BLACK OUTLINE.
2. ALL YELLOW PAVEMENT MARKINGS CONTAIN GLASS BEADS.
3. BLACK MARKINGS DO NOT CONTAIN GLASS BEADS.

**ENHANCED TAXIWAY CENTERLINE  
AND HOLDING POSITION MARKING**

(2) AM  
NO. 8

OGDEN-HINCKLEY AIRPORT  
UNWAY 3-21 AND TAXIWAY  
AVEMENT MAINTENANCE  
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OGDEN UT 844405

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 PAVEMENT MAINTENANCE**  
 3909 AIRPORT RD  
 OGDEN, UT 84405

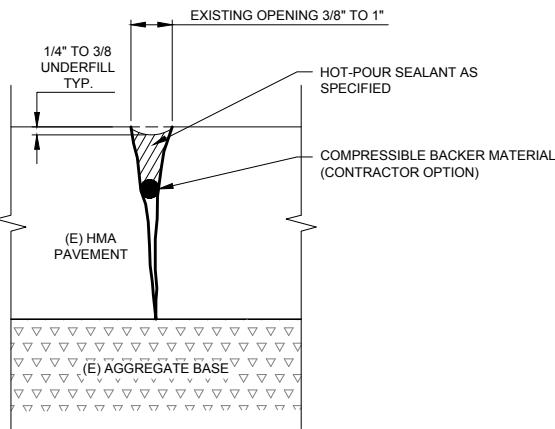
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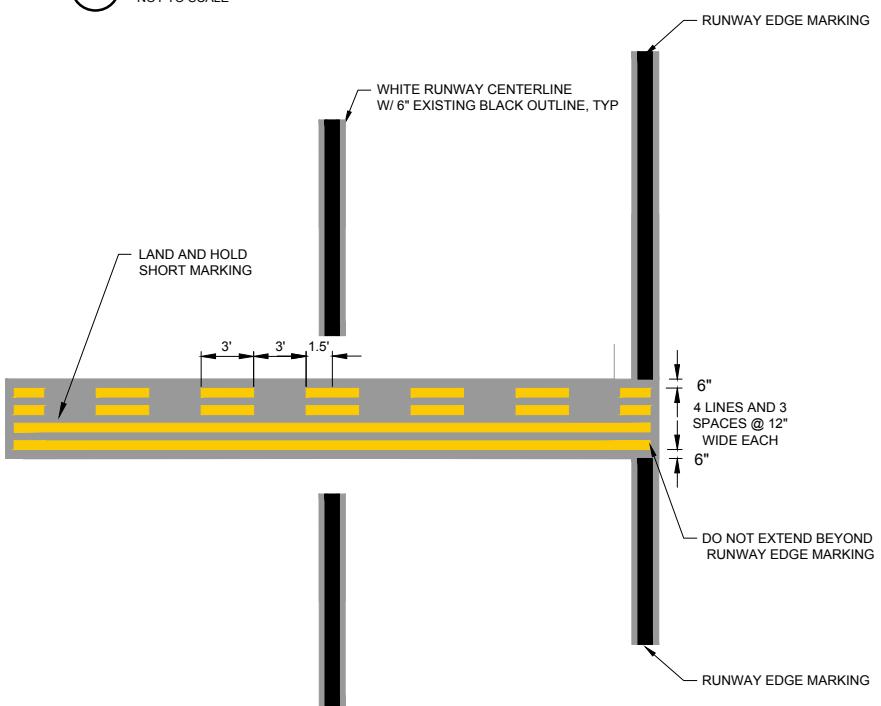
**C-214**



NOTES:

- CLEAN CRACK AND REMOVE LOOSE, NON-COMPRESSIBLE MATERIAL.
- USE HOT LANCE TO ENHANCE ADHESION TO THE EXISTING BITUMINOUS MATERIAL PRIOR TO SEALING.
- SEAL CRACK WITH HOT-POUR SEALANT.
- DO NOT OVER-POUR SEALANT, SQUEEGEE TOP OF CRACK TO REMOVE EXCESS JOINT SEALANT ON THE SURFACE.
- CONTRACTOR WILL BE REQUIRED TO TOP OFF ANY SEALANT THAT SETTLED TO COMPLY WITH THE UNDERFILL REQUIREMENTS. NO EXTRA PAYMENT WILL BE MADE FOR THIS RESEALING AS DESCRIBED HEREIN.
- THIS ITEM WILL BE PAID FOR UNDER MINOR CRACK REPAIR IDENTIFIED IN THE BID FORM.

**1 MINOR CRACK REPAIR  
 (CRACK OPENING FROM 3/8" TO 1")**



NOTE:

- RUNWAY CENTERLINE MARKINGS ARE WHITE W/6" EXISTING BLACK OUTLINE. DIMENSIONS SHOWN EXCLUDE THE BLACK OUTLINE.
- LAND AND HOLD SHORT MARKINGS ARE YELLOW W/6" EXISTING BLACK OUTLINE. DIMENSIONS SHOWN EXCLUDE THE BLACK OUTLINE.
- ALL YELLOW PAVEMENT MARKINGS SHOULD CONTAIN GLASS BEADS.
- BLACK MARKINGS DO NOT CONTAIN GLASS BEADS.
- LAHSO MARKING INTERRUPTS CENTERLINE AND EDGE MARKINGS. DO NOT EXTEND ONTO PAVED SHOULDERS.

**2 LAND AND HOLD SHORT MARKING (LAHSO)**



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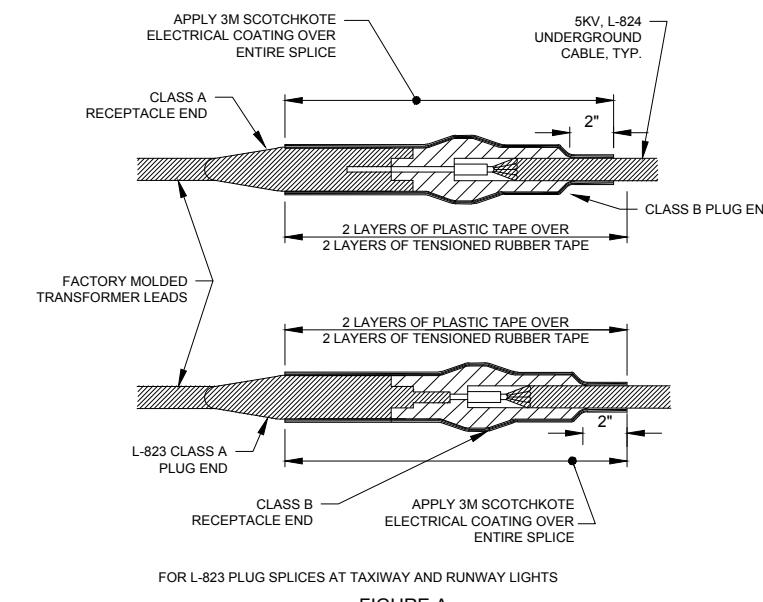
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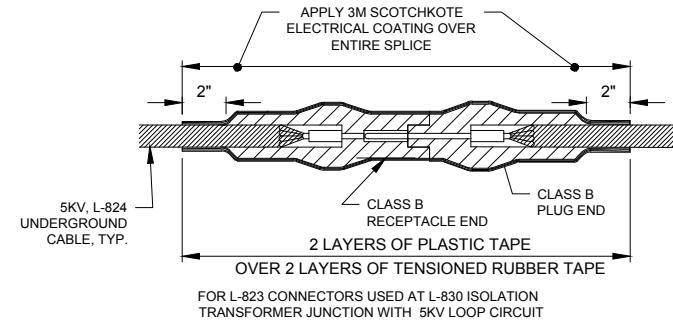
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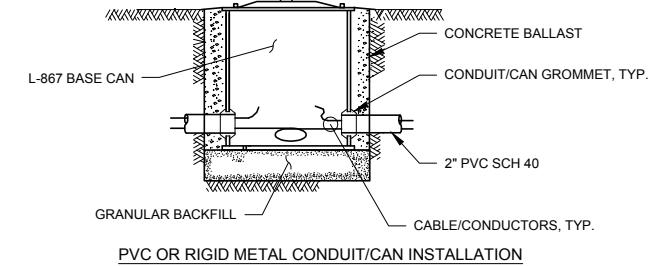


**FIGURE A**

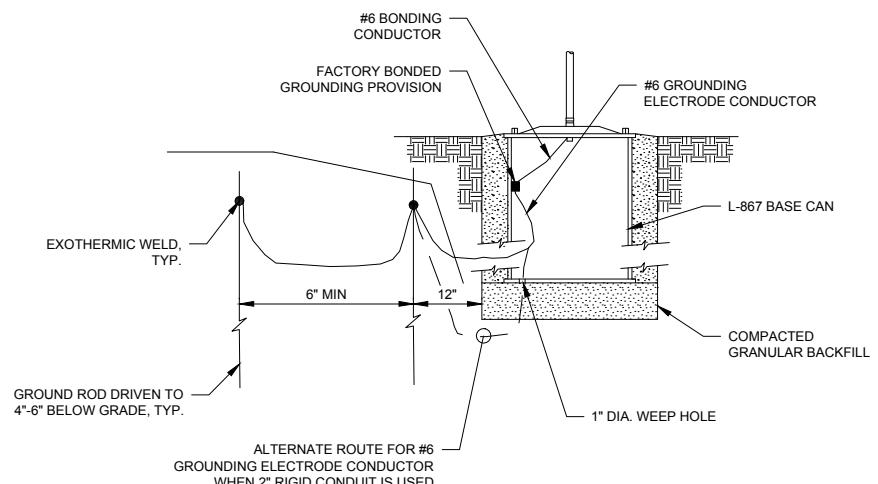


**FIGURE B**

**3 CABLE SPICE WATERPROOFING DETAIL**  
NO SCALE



**1 BASE CAN/CONDUIT HUB DETAIL**  
NO SCALE



**2 L-867 BASE CAN  
STANDARD GROUNDING DETAIL**  
NO SCALE

**GENERAL NOTES**

1. L-823 SPLICES SHALL BE EITHER "COMPLETE" OR "SUPER" KITS.
2. NON-METALLIC COMPONENTS IN THE LIGHT FIXTURE STEMS & BREAKABLE COUPLINGS ARE NOT PERMITTED.
3. L-830 TRANSFORMERS SHALL BE SIZED AS REQUIRED BY THE MANUFACTURER.
4. 5KV, L-824 CABLE SHALL BE AS SHOWN ON THE PLANS.
5. APPLY ELECTRICAL GREASE TO ALL L-823 CLASS A AND B CONNECTORS. ( $\pm 1/32$ " THICK).
6. PROVIDE 3 FEET OF 5KV CABLE SLACK TO EACH ISOLATION TRANSFORMER.
7. ALL FINISHED GRADES SHALL BE FLUSH WITH CONCRETE SLABS AND BALLASTS.
8. ALL HARDWARE SHALL BE STAINLESS STEEL. ALL MACHINE THREADED CONNECTIONS SHALL HAVE ANTI-SEIZING COMPOUND.



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RUNWAY 3-21 AND TAXIWAY  
PAVEMENT MAINTENANCE**  
3909 AIRPORT RD  
OGDEN, UT 84405

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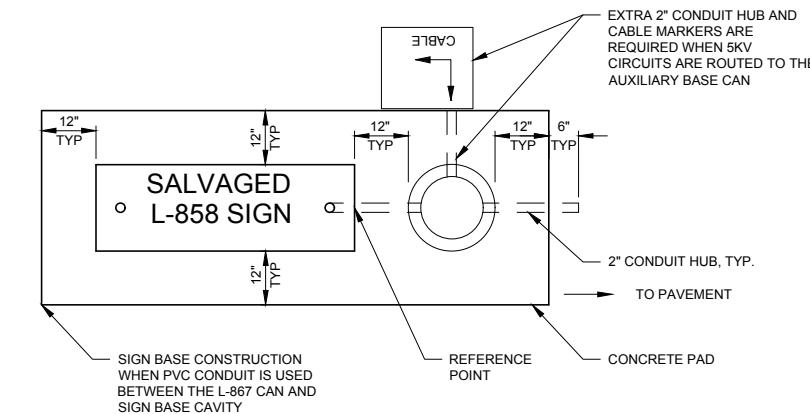
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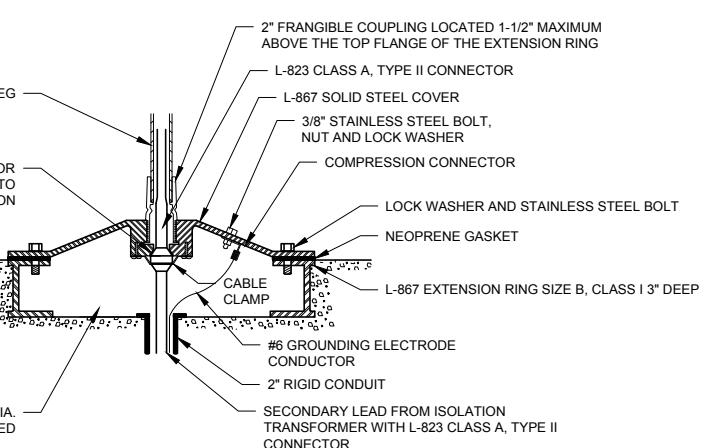
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**1 SIGN BASE DETAIL - PLAN VIEW**  
NO SCALE



**2 SIGN BASE CAVITY DETAIL A**  
NO SCALE

